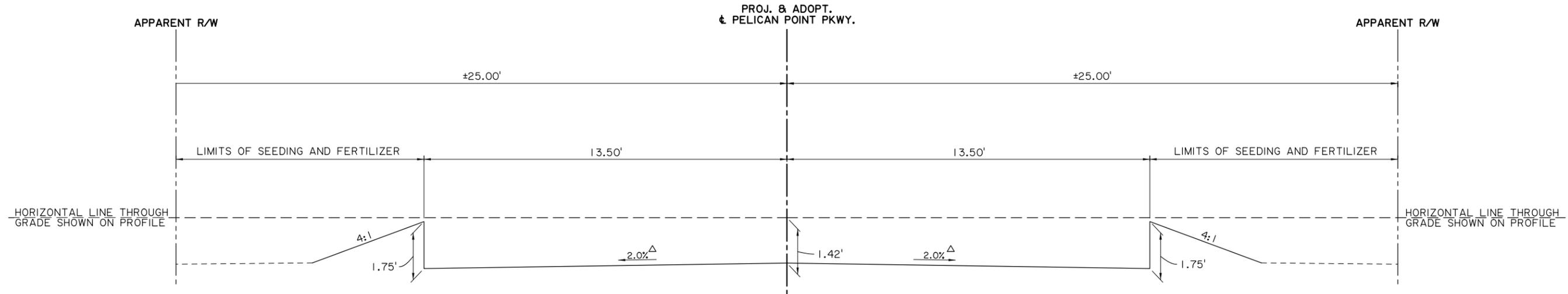


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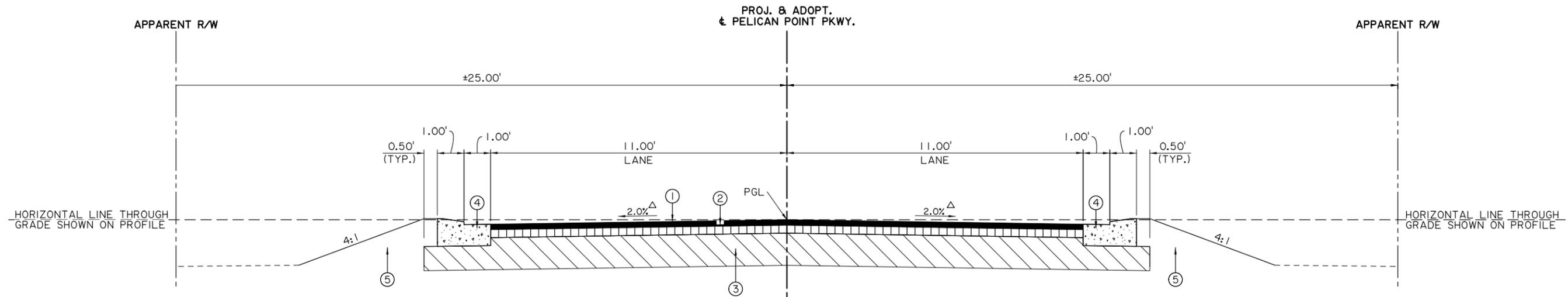
8/9/2019

FINAL PLANS

P:\Y-2019\2019-0266\ENG\Drawings\FP_100_PCT\002_Typical Section.dgn



TYPICAL GRADING SECTION (N.T.S.)
APPLIES STA. 11+24.32 TO STA. 12+35.00

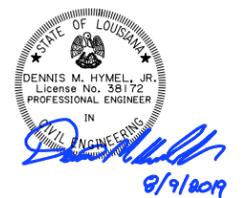


TYPICAL FINISHED SECTION (N.T.S.)
APPLIES STA. 11+24.32 TO STA. 12+35.00

LEGEND

- ① REQ'D. 2" THICK ASPHALT CONCRETE WEARING COURSE (LEVEL 2)
- ② REQ'D. 3" THICK ASPHALT CONCRETE BINDER COURSE (LEVEL 2)
- ③ REQ'D. CLASS II BASE COURSE (12" THICK) (ASPHALT CONCRETE BASE ON EMBANKMENT LAYER) *
- ④ REQ'D. 2' COMBINATION CONCRETE CURB & GUTTER (MOUNTABLE) (8" THICK AT GUTTER)
- ⑤ EMBANKMENT

- * CLASS II BASE COURSE THICKNESS IS 9" BELOW CURB AND GUTTER.
- Δ 2.0% REPRESENTS AVERAGE CROSS SLOPE, SEE GRAPHICAL GRADES FOR MORE DETAILS.



SHEET NUMBER	2
ASCE	ASCENSION
PARISH	ASCENSION
CONTROL SECTION	CONTROL SECTION
DESIGNED BY	A. FREDERIC
CHECKED BY	D. HYMEL
DRAWN BY	L. BOURG
CHECKED BY	P. OLIVIER
SERIES NUMBER	1 OF 1
PROJECT	EAD-19-004
NO.	
DATE	
REVISION OR CHANGE ORDER	
DESCRIPTION	
BY	

TYPICAL SECTIONS & DETAILS

PELICAN POINT PKWY. CROSS DRAIN REPAIR

T. BAKER SMITH

LEGEND - EXISTING TOPOGRAPHY

CONTROL POINT	△	GAS LINE	—G—G—
TEMPORARY BENCH MARK	⊗	GAS METER	⊗
PHOTO TARGET	⊗	GAS SERVICE (NO METER)	⊗
PAVEMENT EDGE	—	GAS REGULATOR	⊗
SHOULDER EDGE	—	GAS RISER	⊗
SLOPE TOE	—	GAS TEST BOX	⊗
GUARDRAIL TOP	—	GAS VALVE	⊗
HIGH BANK	—	GAS LINE/CASING	—G—G—
WATER'S EDGE	—	GAS VENT	⊗
HIGH WATER MARK	+	RAILROAD MILEPOST	—
BOX CULVERT	—	RAILROAD SIGNAL	—
PIPE CULVERT	—	RAILROAD SWITCH	—
CATCH BASIN TOP (ROUND)	⊗	RAILROAD TRACK	—
DROP INLET TOP (ROUND)	⊗	RR TRAFFIC SIGNAL BOX	—
DRAINAGE MANHOLE TOP	⊗	SEWER LINE	—S—S—
LEVEE TOP	—	SEWER MANHOLE TOP	⊗
DITCH CENTERLINE	—	SEWER BLOWOUT VALVE	⊗
TREE	—	SEWER CLEANOUT	⊗
WOODS EDGE	—	SEPTIC TANK	⊗
MARSH LINE	—	SEWER PUMP (PRIVATE)	⊗
SWAMP LINE	—	SEWER TREATMENT (INDIVIDUAL)	⊗
TREE CLUSTER	—	FEDERAL AID MARKER	⊗
HEDGE	—	TRAFFIC CONTROLLER BOX	⊗
BUSH	—	TRAFFIC COUNTER	⊗
TREE LINE	—	TRAFFIC SIGNAL	⊗
FENCE LINE	—	TRAFFIC SIGNAL SUPPORT POLE	⊗
GATE	—	LIGHT POLE	⊗
CATTLE GUARD	—	LIGHT PEDESTAL	⊗
PROPERTY CORNER	⊗	LIGHT POWER VAULT	⊗
RIGHT OF WAY MONUMENT	⊗	TRAFFIC SIGN	⊗
SECTION CORNER	⊗	PARKING METER	⊗
FENCE CORNER	⊗	TELEPHONE POLE	⊗
TELEVISION CABLE	—TV—	TELEPHONE BOOTH	⊗
TELEVISION PEDESTAL	⊗	TELE CROSS CONNECT BOX	⊗
POWER POLE	⊗	TELEPHONE PEDESTAL	⊗
DEADMAN	⊗	TELEPHONE PRESSURE BOX	⊗
POWER LINE	—P—	WATER LINE	—W—W—
POWER JUNCTION BOX	⊗	WATER LINE/CASING	—W—W—
POWER VAULT	⊗	WATER CLEANOUT	⊗
TRANSFORMER	⊗	WATER METER	⊗
COMBINATION POLE	⊗	WATER VALVE	⊗
POWER DROP	⊗	WATER VALVE VAULT	⊗
PIPELINE	—	WATER WELL	⊗
PIPELINE VENT	⊗	FIRE HYDRANT	⊗
PIPELINE REGULATOR	⊗	BILLBOARD	⊗
GAS WELL	⊗	FUEL PUMP	⊗
HAY BALES	—	POST	⊗
SILT FENCE	—SF—	SIGN POST	⊗
INLET SILT TRAP	⊗	STORAGE TANK (ROUND)	⊗
		GRAVE	⊗
		MAILBOX	⊗
		ORNAMENTAL LIGHT	⊗
		FLAG POLE	⊗

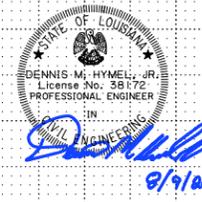
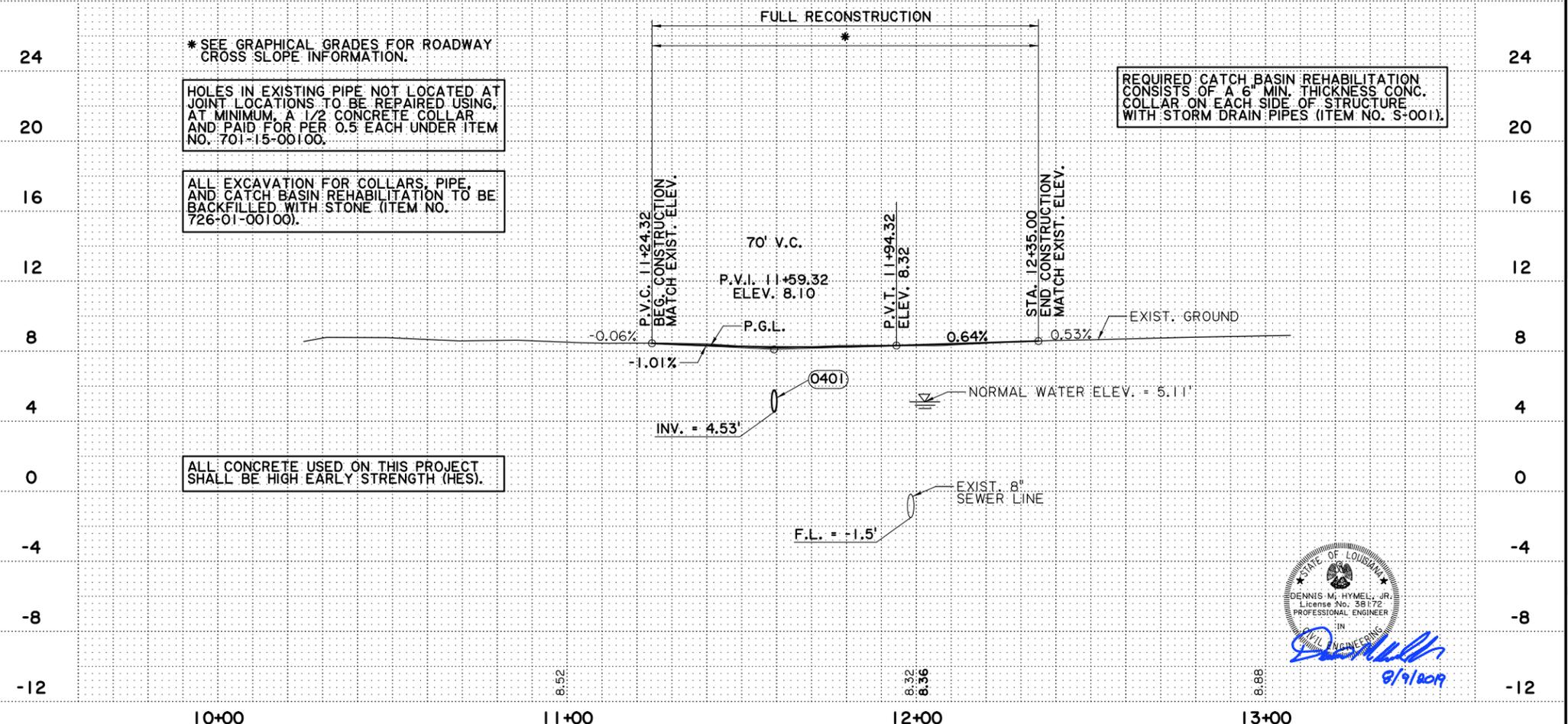
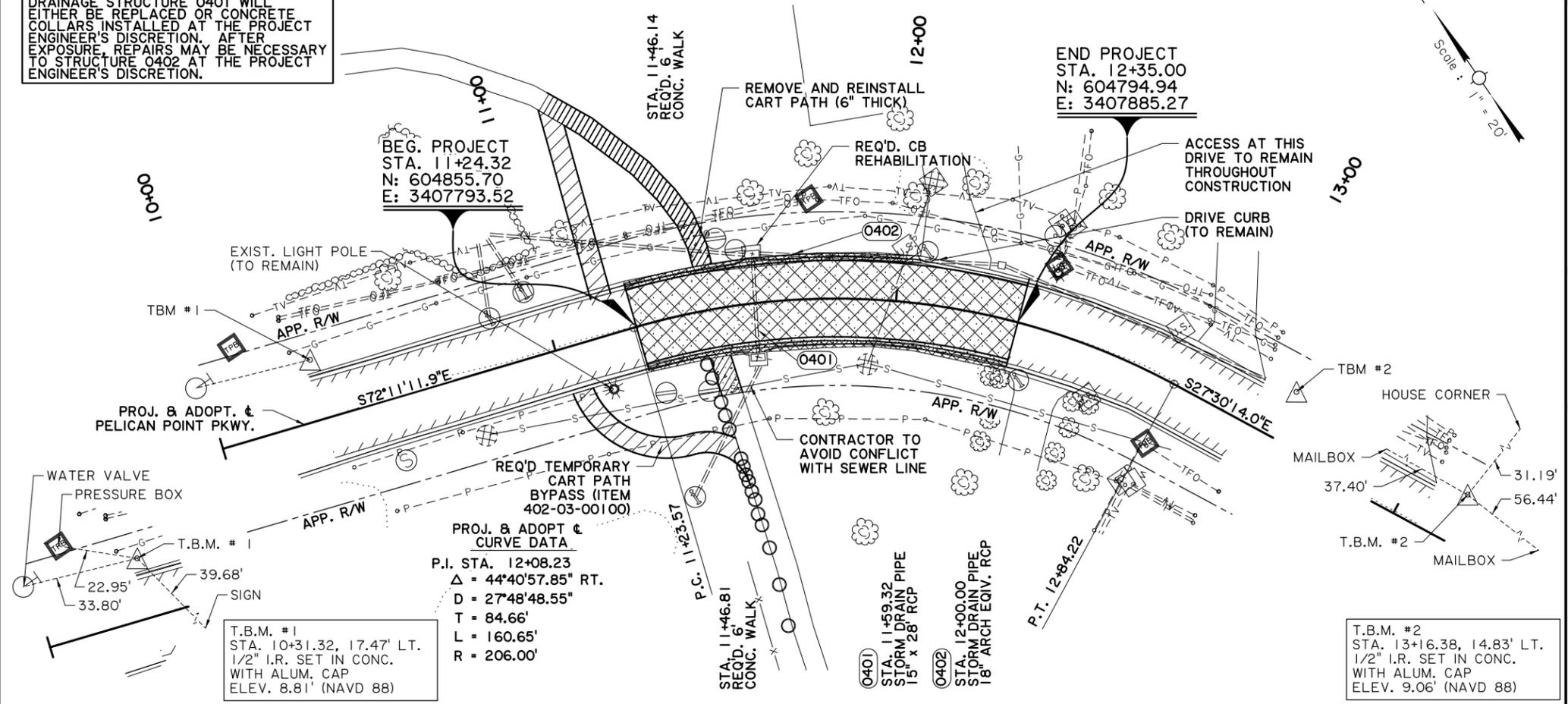
LEGEND

REQ'D ASPHALT PAVEMENT	▨
REQ'D CONCRETE PAVEMENT	▨
REMOVAL OF CONC. WALKS & DRIVES	▨
REMOVAL OF SURFACING & BASE	▨
EXIST. ROADWAY TO REMAIN	▨
REMOVAL OF CURB	▨

NOTES:

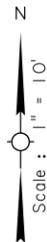
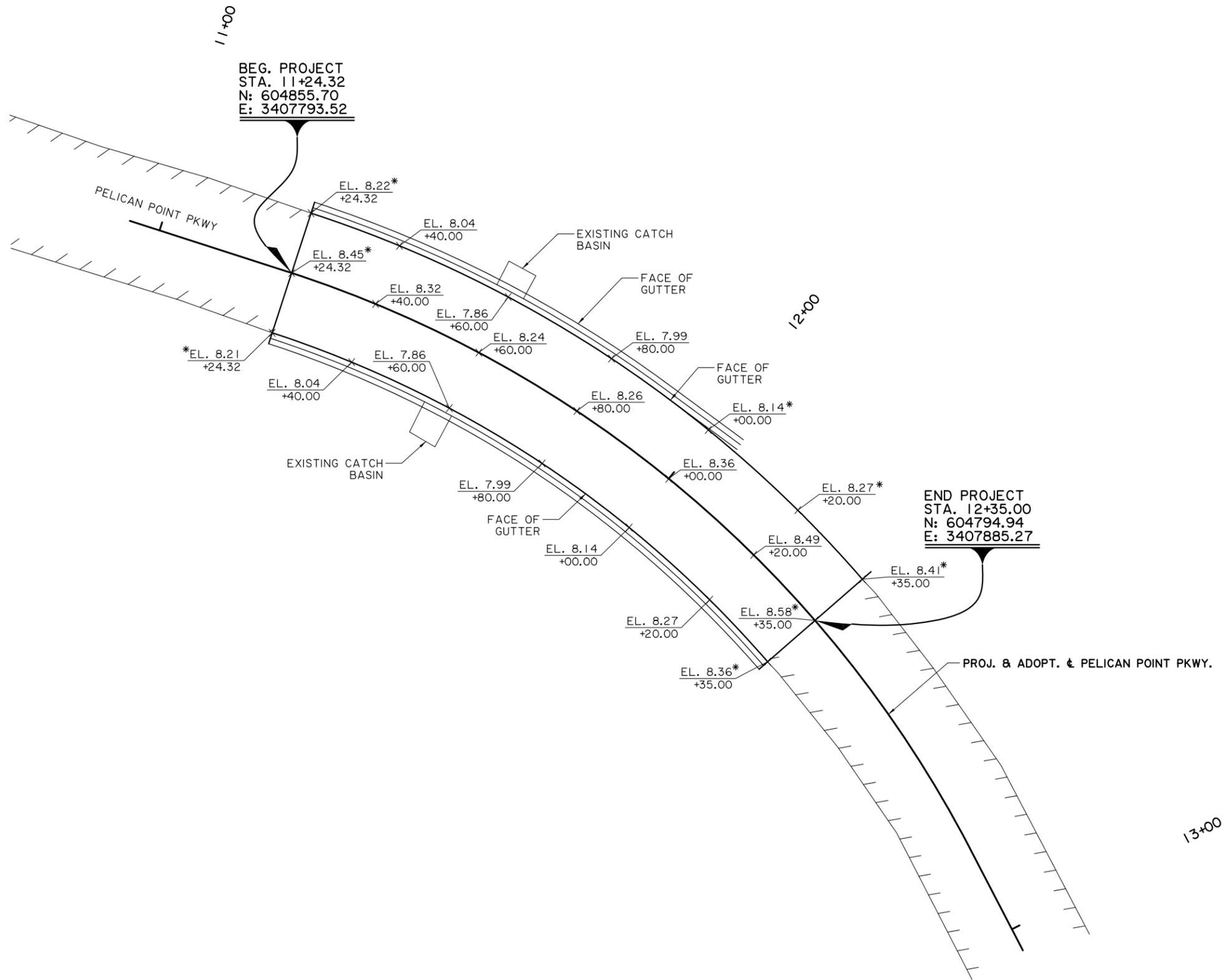
- UPON EXPOSURE OF THE STORM DRAIN PIPES, PROJECT ENGINEER SHALL BE NOTIFIED TO INSPECT THE PIPE CONDITION AND TO DETERMINE IF CONCRETE COLLARS WILL BE INSTALLED OR IF REPLACEMENT OF PIPES ARE NECESSARY. THE APPROPRIATE PAY ITEMS WILL BE UTILIZED. SOME PAY ITEMS MAY NOT BE UTILIZED.
- CONTRACTOR TO MAINTAIN ONE LANE OF TRAFFIC AT ALL TIMES AND SHALL INSTALL TEMPORARY TRAFFIC CONTROL MEASURES AS NECESSARY FOR THIS OPERATION.
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL RESIDENTIAL DRIVES AND GOLF COURSE CART PATH AT ALL TIMES.
- DRIVEWAYS, IF DAMAGED, SHALL BE REPLACED IN-KIND AND INCLUDE DECORATIVE BRICK AREAS. ALL REQUIREMENTS FOR DRIVEWAY REPLACEMENT INCLUDED IN ITEM NO. 706-03-00200.
- IF TEMPORARY MAILBOXES ARE REQUIRED DURING CONSTRUCTION, CONTRACTOR SHALL INSTALL AS NEEDED (INCLUDED IN ITEM NO. 727-01-00100).
- CONTRACTOR'S METHODS OF DE-WATERING THE STORM DRAIN SYSTEM, INCLUDING TEMPORARY BLOCKING OF PIPES TO BE INCLUDED UNDER ITEM NO. S-002, PER LUMP SUM.
- ALL SIGNS LOCATED WITHIN THE PROJECT LIMITS SHALL BE REMOVED, STORED, AND RE-INSTALLED UPON COMPLETION OF PROJECT.

DRAINAGE STRUCTURE 0401 WILL EITHER BE REPLACED OR CONCRETE COLLARS INSTALLED AT THE PROJECT ENGINEER'S DISCRETION. AFTER EXPOSURE, REPAIRS MAY BE NECESSARY TO STRUCTURE 0402 AT THE PROJECT ENGINEER'S DISCRETION.

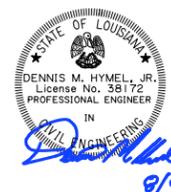


SHEET NUMBER	4
DESIGNED	A. FREDERIC
CHECKED	D. HYMEL
PARISH	ASCENSION
CONTROL SECTION	L. BOURG
Detailed	P. OLIVIER
CHECKED	
PARISH PROJECT	EAD-19-004
SERIES NUMBER	1 OF 1
NO.	
DATE	
REVISION OR CHANGE ORDER DESCRIPTION	
BY	
PLAN AND PROFILE	PELICAN POINT PKWY. CROSS DRAIN REPAIR
T. BAKER SMITH	

FINAL PLANS

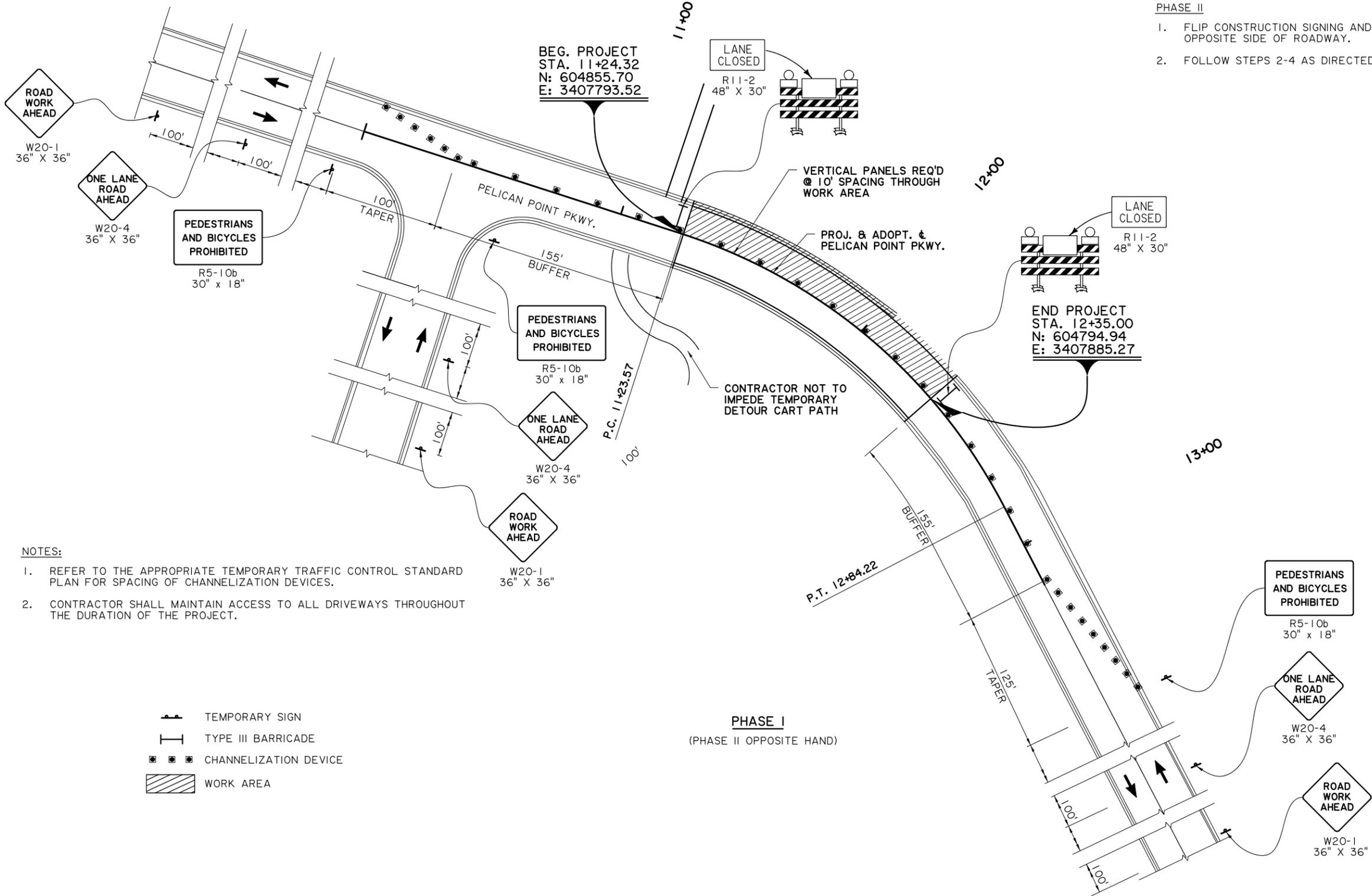


* MATCH EXISTING ROADWAY ELEVATION



T. BAKER SMITH
8/9/2019

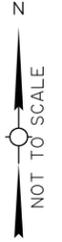
SHEET NUMBER		5	
DESIGNED	A. FREDERIC	PARISH	ASCENSION
CHECKED	D. HYMEL	CONTROL SECTION	
DETAILED	L. BOURG	PARISH PROJECT	EAD-19-004
CHECKED	P. OLIVIER		
SERIES NUMBER	1 OF 1		
NO.	DATE	REVISION OR CHANGE ORDER DESCRIPTION	BY
GRAPHICAL GRADES PELICAN POINT PKWY. CROSS DRAIN REPAIR			
T. BAKER SMITH			



NOTES:

- REFER TO THE APPROPRIATE TEMPORARY TRAFFIC CONTROL STANDARD PLAN FOR SPACING OF CHANNELIZATION DEVICES.
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS THROUGHOUT THE DURATION OF THE PROJECT.

- TEMPORARY SIGN
- TYPE III BARRICADE
- CHANNELIZATION DEVICE
- WORK AREA



SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE I

- INSTALL CONSTRUCTION SIGNING AND CHANNELIZATION DEVICES.
- REMOVE EXISTING WESTBOUND TRAVEL LANE AND INSPECT CONDITION OF THE EXISTING CATCH BASIN & PIPES WITH PROJECT ENGINEER.
- REPAIR OR REPLACE DRAINAGE STRUCTURES AS NEEDED.
- BUILD NEW ROADWAY ACCORDING TO THE TYPICAL SECTION AS SHOWN ON SHEET 3.

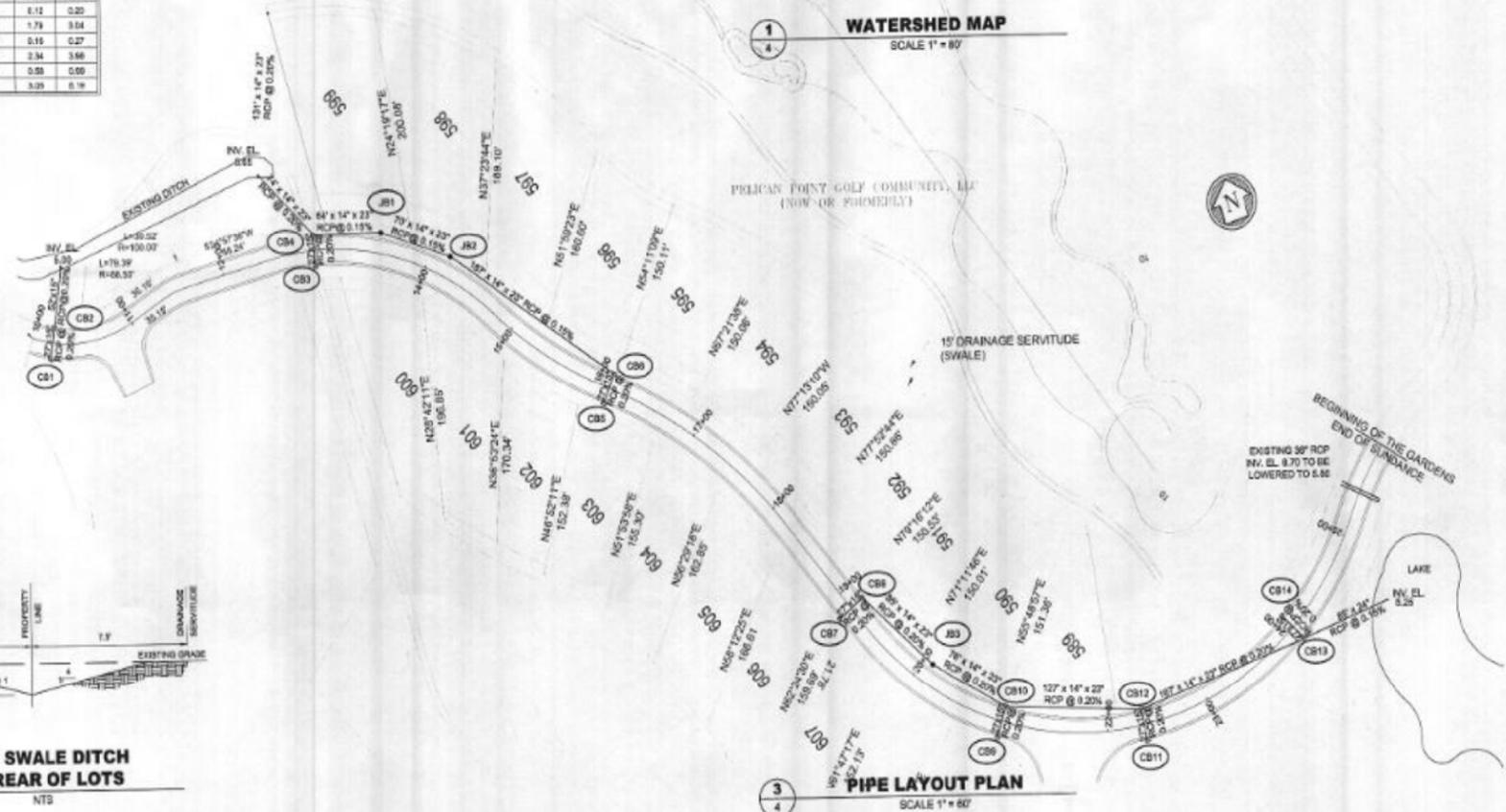
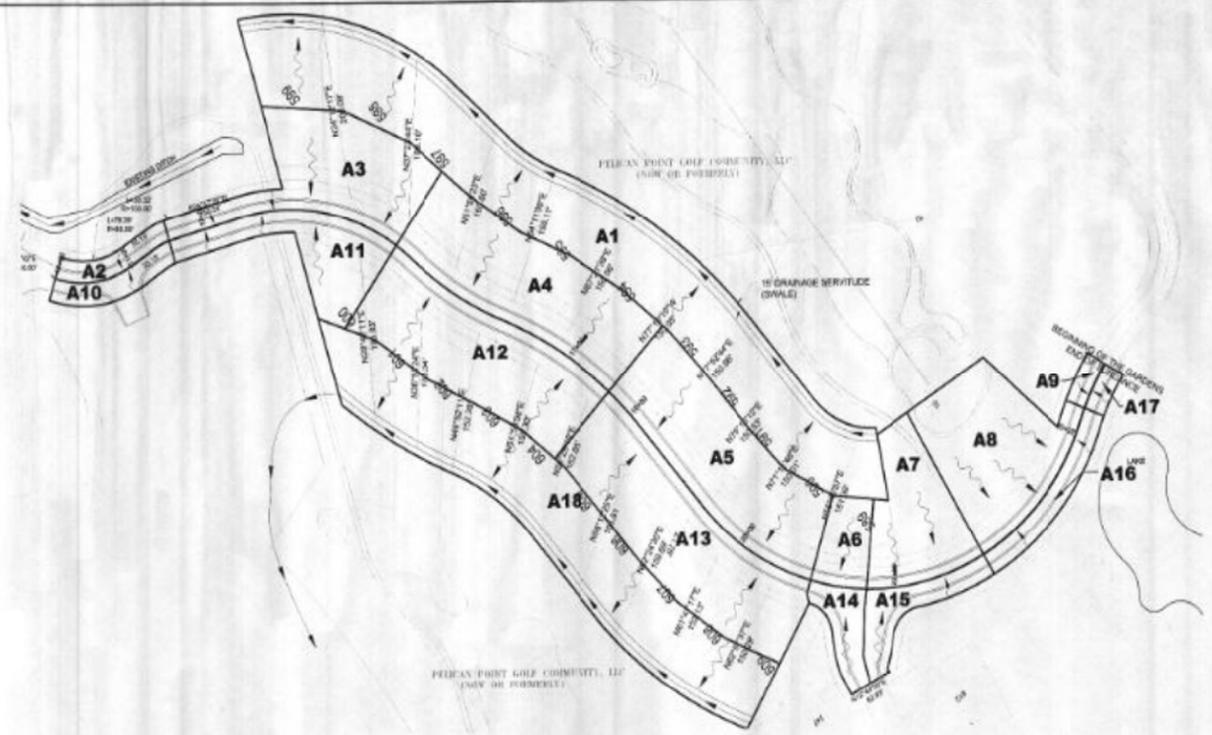
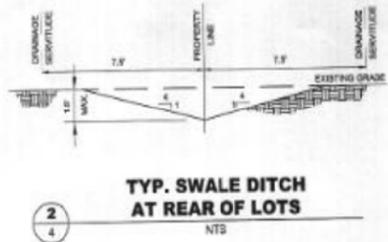
PHASE II

- FLIP CONSTRUCTION SIGNING AND CHANNELIZATION TO OPPOSITE SIDE OF ROADWAY.
- FOLLOW STEPS 2-4 AS DIRECTED IN PHASE I.

SHEET NUMBER	6
DESIGNED BY	A. FREDERIC
CHECKED BY	D. HYMEL
PARISH	ASCENSION
CONTROL SECTION	L. BOURG
PARISH PROJECT	EAD-19-004
Detailed By	P. OLIVIER
Checked By	
Series Number	1 OF 1
Revision or Change Order Description	
No.	
Date	
SUGGESTED SEQUENCE OF CONSTRUCTION & MIN. SIGNING	
PELICAN POINT PKWY. CROSS DRAIN REPAIR	
T. BAKER SMITH	

DESIGNATION	AREA (Ac.)	Q (cfs)
A1	1.71	2.91
A2	0.06	0.14
A3	0.68	0.88
A4	0.73	1.34
A5	0.72	1.22
A6	0.14	0.24
A7	0.39	0.68
A8	0.26	0.36
A9	0.03	0.06
A10	0.09	0.15
A11	0.32	0.54
A12	0.71	1.21
A13	0.81	1.38
A14	0.12	0.20
A15	0.16	0.27
A16	0.19	0.32
A17	0.03	0.05
A18	1.16	1.87

FROM	TO	SIZE (in.)	LENGTH (ft.)	AREA (Ac.)	Q (cfs)
CB1	CB2	15	27	0.05	0.14
CB2	DITCH	15	32	0.17	0.29
CB3	CB4	15	27	0.71	1.21
CB4	J82	14x23	157	1.44	2.45
J82	J81	14x23	79	1.44	2.45
J81	CB4	14x23	94	1.44	2.45
CB3	CB4	15	27	0.32	0.54
CB4	DITCH	14x23	79	2.34	3.99
SWAILE	DITCH	14x23	131	1.71	2.91
CB7	CB6	15	27	0.81	1.38
CB6	J83	14x23	85	1.53	2.60
J83	CB10	14x23	76	1.53	2.60
CB9	CB10	15	27	0.12	0.20
CB10	CB12	14x23	127	1.79	3.04
CB11	CB12	15	27	0.16	0.27
CB12	CB13	14x23	167	2.34	3.66
CB14	CB13	15	27	0.08	0.14
CB13	LAKR	24	89	3.39	5.19



- CONSTRUCTION NOTES:
- PROPERTY IS LOCATED IN FLOOD ZONE C IN ACCORDANCE WITH FLOOD INSURANCE RATE MAPS 220013-00120C OF ASCENSION PARISH, LOUISIANA.
 - 100-YEAR BASE FLOOD ELEVATION 8.0 NVGD.
 - FLAWS DERIVED USING 1.70 cfs PER ACRE FOR POST DEVELOPED CONDITIONS WITHIN THE SUBDIVISION.
 $Q = C I A$ (RATIONAL METHOD)
 $I = 3.4 \text{ in./hr.}$
 $C = 0.30$ (UNDEVELOPED)
 $C = 0.50$ (POST DEVELOPED)
 - ALL STORM DRAINAGE PIPE TO BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM C-76 CLASS III (CIRCULAR PIPE) OR ASTM C-806 (ARCH PIPE) WITH RUBBER GASKET JOINTS.
 - ALL DRAINAGE BOX CASTINGS SHALL CONFORM TO A.S.T.M A48-48 CLASS 30 FOR GRAY IRON CASTING.
 - ALL EXCESS EXCAVATION IS TO SPREAD ON-SITE AT THE DIRECTION OF THE PROJECT ENGINEER.
 - IT SHALL BE THE RESPONSIBILITY OF THE ROADWAY CONTRACTOR TO GRADE EACH LOT SO THAT THE STORM DRAINAGE RUNOFF CONFORMS TO THIS APPROVED DRAINAGE LAYOUT. ULTIMATELY, AS PART OF THE HOUSE CONSTRUCTION, IT SHALL BE THE RESPONSIBILITY OF THE LOT OWNER OR HIS/HER REPRESENTATIVE TO GRADE EACH LOT SO THAT THE STORM DRAINAGE RUNOFF CONFORMS TO THIS APPROVED DRAINAGE LAYOUT.
 - THE SWALE DITCHES ARE TO BE CENTERED IN THE 15' SERVITUDE AT THE REAR OF THE LOTS.

LEGEND

SYMBOL	DESCRIPTION
CB1	STRUCTURE DESIGNATION
~	SURFACE GRADING
▭	PROPOSED CATCH BASIN
—	EXISTING CONTOURS
—	PROPOSED SWALE DITCH

SUNDANCE
PELICAN POINT 10th FILING
 Applied Engineering Services, LLC.

STORMWATER

4 OF 9

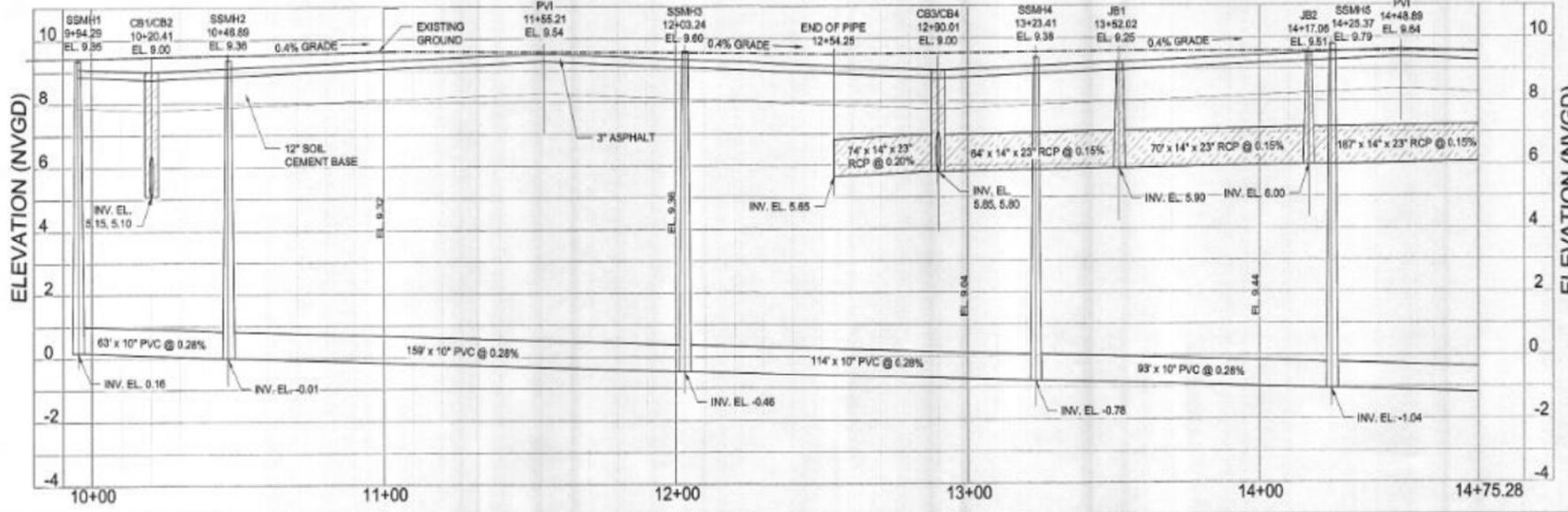
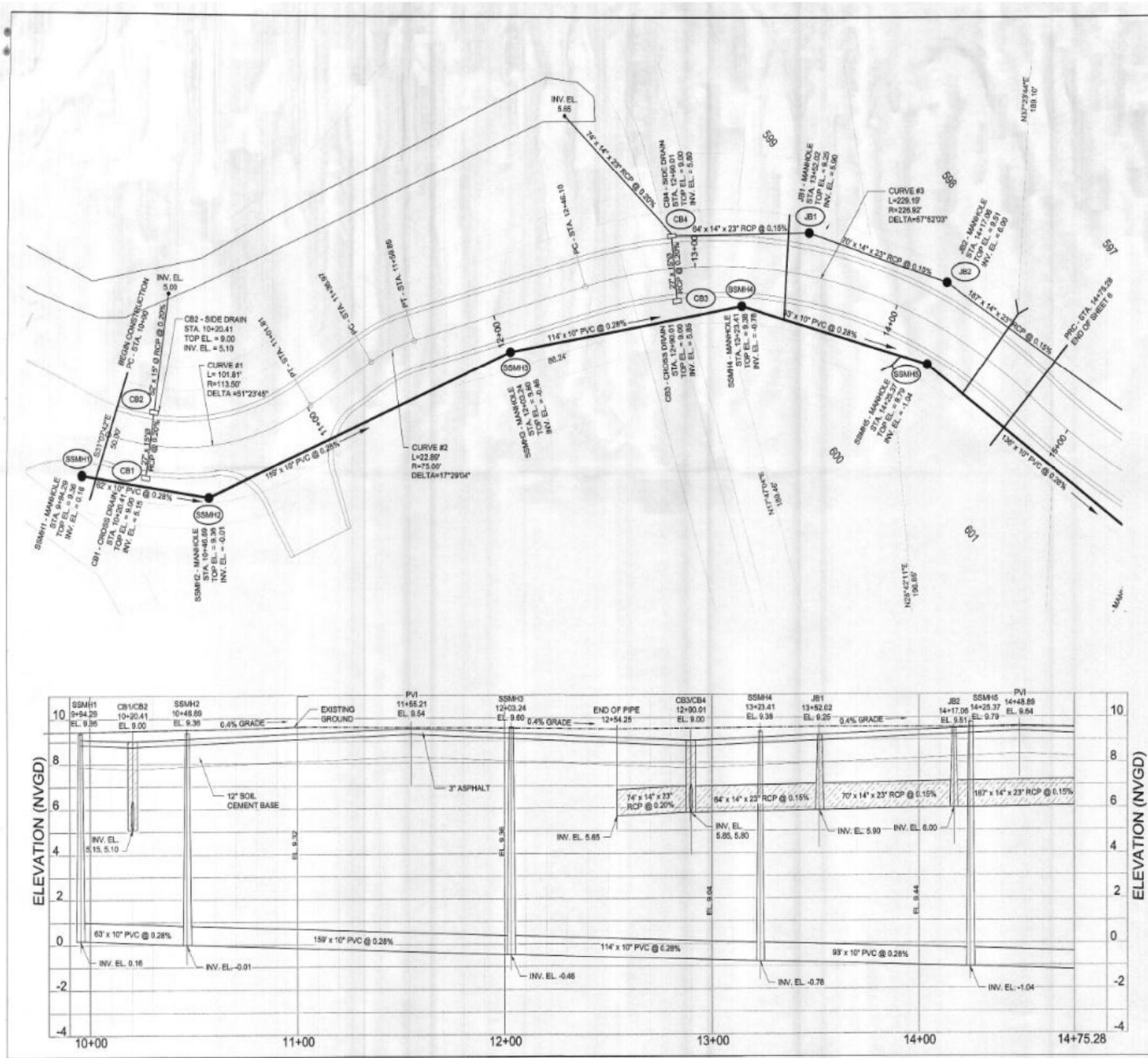
DESIGNED BY: J. BECOTEAU
 CHECKED BY: J. BAY
 PROJECT NO: AES #2002-027
 DATE: APR. 8, 2002

AS-BUILT

FOR INFORMATIONAL PURPOSES ONLY.



SHEET NUMBER	8
DESIGNED BY	A. FREDERIC
CHECKED BY	D. HYMEL
PARISH	ASCENSION
CONTROL SECTION	L. BOURG
CHECKED BY	P. OLIVIER
PARISH PROJECT	EAD-19-004
SERIES NUMBER	1 OF 2
REVISION OR CHANGE ORDER	BY
NO.	DATE
AS-BUILT PLANS	
PELICAN POINT PKWY. CROSS DRAIN REPAIR	
T. BAKER SMITH	



CONSTRUCTION NOTES:

GENERAL NOTES:

VERTICAL SCALE: 1"=2'-0"

HORIZONTAL SCALE: 1"=20'-0"

No. Revisions / Submitters Date

SUNDANCE
PELICAN POINT 10th FILING
 Golf Community

Applied Engineering Services, LLC.

DRAWING TITLE: **STATIONS 10+00 to 14+75.28**

DATE: APR. 8. 2002

PROJECT NO: AES #2002-027

AS NOTED ON DRAWING

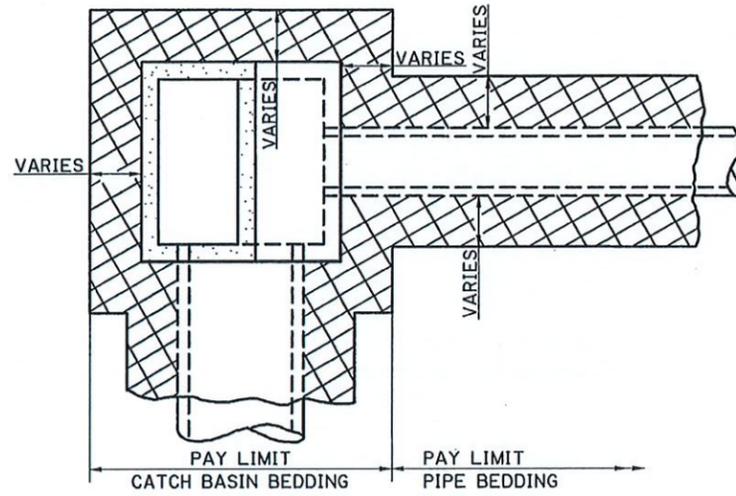
6 OF 9

AS-BUILT FOR INFORMATIONAL PURPOSES ONLY.

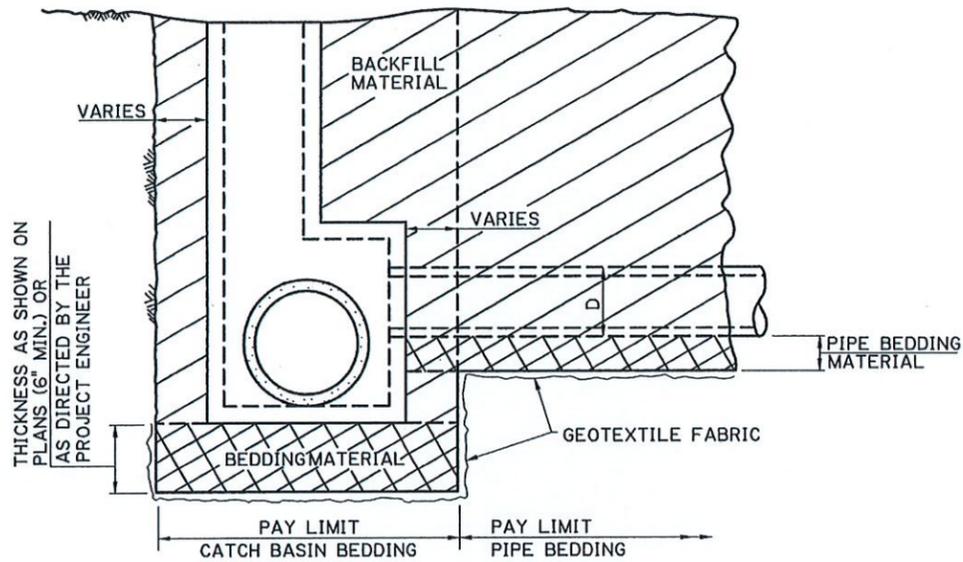


SHEET NUMBER	9
DESIGNED	A. FREDERIC
CHECKED	D. HYMEL
PARISH	ASCENSION
CONTROL SECTION	L. BOURG
CHECKED	P. OLIVIER
PARISH PROJECT	EAD-19-004
SERIES NUMBER	2 OF 2
REVISION OF CHANGE ORDER	BY
NO.	DATE
AS-BUILT PLANS	
PELICAN POINT PKWY. CROSS DRAIN REPAIR	
T. BAKER SMITH	

TYPICAL CATCH BASIN AND STORM SEWER PIPE INSTALLATION WITH BEDDING MATERIAL



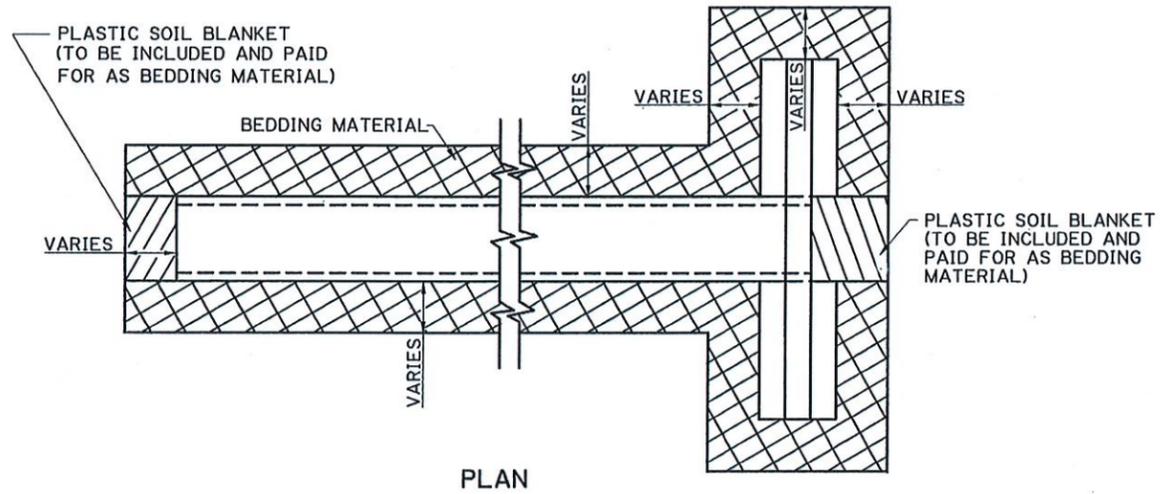
PLAN OF TYPICAL CATCH BASIN (MANHOLE OR JUNCTION BOX) AND PIPE



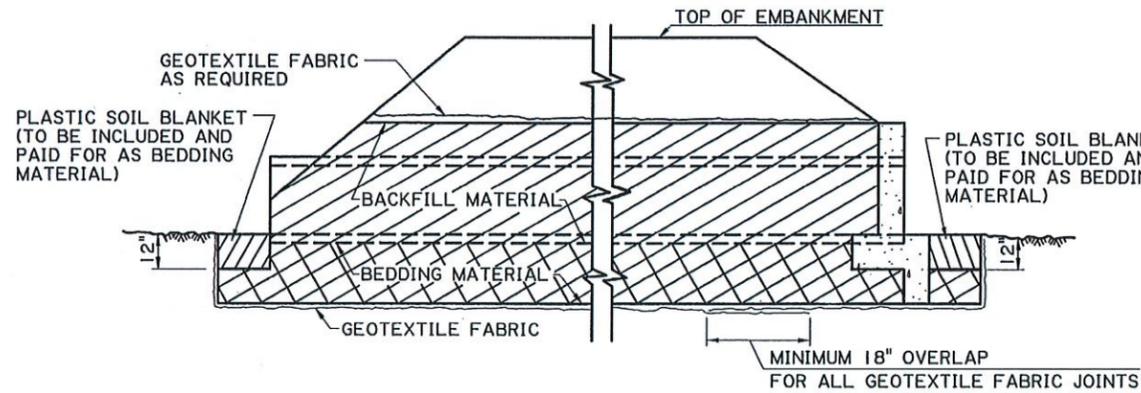
PROFILE OF TYPICAL CATCH BASIN (MANHOLE OR JUNCTION BOX) AND PIPE

THICKNESS AS SHOWN ON PLANS (6" MIN.) OR AS DIRECTED BY THE PROJECT ENGINEER

TYPICAL CROSS DRAIN INSTALLATION WITH BEDDING MATERIAL

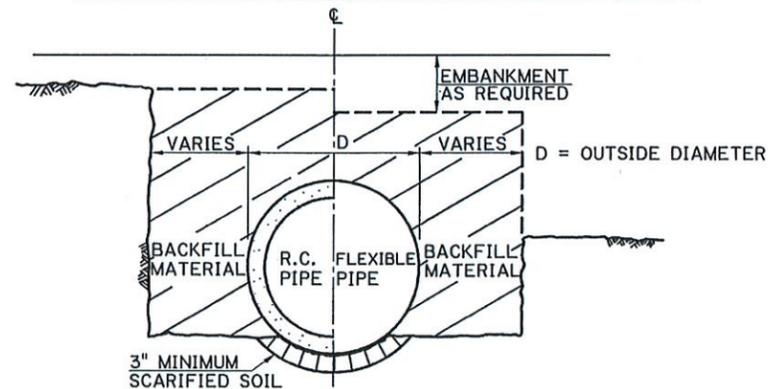


PLAN



PROFILE

TYPICAL PIPE INSTALLATION WITHOUT BEDDING MATERIAL



SECTION THRU TRENCH/EMBANKMENT

HALF-SECTION SHOWING TOP OF PIPE GREATER THAN 12" BELOW ORIGINAL GROUND (TRENCH INSTALLATION)

HALF-SECTION SHOWING TOP OF PIPE ABOVE ORIGINAL GROUND (EMBANKMENT INSTALLATION)

GENERAL NOTES

1. CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LA DOTD STANDARD SPECIFICATIONS
2. STANDARD DOTD PIPE INSTALLATION, BEDDING, BACKFILL (TYPE "A" OR "B") AND TRENCH WIDTH ARE DEFINED IN SECTIONS 701 AND 726 OF THE LA DOTD STANDARD SPECIFICATIONS.
3. THE NEED AND/OR THE THICKNESS OF BEDDING MATERIAL WILL BE DETERMINED BY THE GEOTECHNICAL SECTION AND WILL BE SHOWN ON THE PLANS. ADDITIONAL BEDDING MATERIAL MAY BE REQUIRED BY THE PROJECT ENGINEER AT NO COST TO THE CONTRACTOR.
4. THE DETAILS ON THIS SHEET DEPICT PAY LIMITS FOR BEDDING MATERIALS. THE BEDDING MATERIAL PAY QUANTITIES ARE TO BE BASED ON THE THEORETICAL NET SECTION WITH NO PIPE DEDUCTIONS.
5. THE BACKFILL IS TO BE MEASURED AND PAID IN ACCORDANCE WITH SECTION 701 OF LA DOTD STANDARD SPECIFICATIONS.
6. BEDDING SHOWN ON THIS STANDARD PLAN CONFORMS TO THE CURRENT AASHTO SPECIFICATIONS.
7. FLEXIBLE PIPE CONSISTS OF ALL CORRUGATED METAL AND PLASTIC PIPE.
8. REINFORCED CONCRETE PIPE AND FLEXIBLE PIPE ARE SHOWN AS TYPICAL STRUCTURES. DETAILS FOR REINFORCED CONCRETE BOX, REINFORCED CONCRETE PIPE ARCH, CORRUGATED METAL PIPE ARCH AND CORRUGATED STRUCTURAL PLATE STRUCTURES ARE SIMILAR.
9. MINIMUM COVER IS 12" FOR ALL PIPE. THERE IS NO MINIMUM COVER REQUIREMENT FOR RCB.

SHEET NUMBER	101
DESIGNED	JDK
CHECKED	JDK
DATE	8-22-07
REVISION TO COMPLY WITH CURRENT SPECIFICATIONS	JDK
ADDED FINAL AND INITIAL BACKFILL	JDK
REDARAFTED, REVISED FOR TYPE A & B BACKFILL	JDK
DATE	7-11-05
REVISION DESCRIPTION	BY
DATE	8-22-07
APPROVED BY	DATE
CHIEF ENGINEER	8-22-07
<p>STATE OF LOUISIANA</p> <p>DENNIS M. HYMEL, JR.</p> <p>License No. 38172</p> <p>PROFESSIONAL ENGINEER</p> <p>IN</p> <p>CIVIL ENGINEERING</p> <p>8/9/2009</p>	
<p>REVISION TO COMPLY WITH CURRENT SPECIFICATIONS</p> <p>ADDED FINAL AND INITIAL BACKFILL</p> <p>REDARAFTED, REVISED FOR TYPE A & B BACKFILL</p>	
<p>DESIGNED: JDK</p> <p>CHECKED: JDK</p> <p>DATE: 8-22-07</p>	
<p>REVISION DESCRIPTION</p> <p>BY: JDK</p> <p>DATE: 8-22-07</p>	
<p>APPROVED BY: [Signature]</p> <p>DATE: 8-22-07</p>	
<p>STANDARD PLAN</p> <p>BM-01</p>	
<p>HYDRAULICS SECTION</p>	
<p>BEDDING AND BACKFILL FOR DRAINAGE STRUCTURES</p>	

THESE STANDARD PLANS HAVE BEEN PROPERLY EXAMINED BY ME, THE UNDERSIGNED PROFESSIONAL ENGINEER. I HAVE DETERMINED THAT THESE PLANS COMPLY WITH ALL APPLICABLE CODES AND HAVE BEEN PROPERLY ADAPTED TO USE ON THIS PROJECT.

TYPICAL PIPE INSTALLATION WITH BEDDING MATERIAL

THESE STANDARD PLANS HAVE BEEN PROPERLY EXAMINED BY ME, THE UNDERSIGNED PROFESSIONAL ENGINEER. I HAVE DETERMINED THAT THESE PLANS COMPLY WITH ALL APPLICABLE CODES AND HAVE BEEN PROPERLY ADAPTED TO USE ON THIS PROJECT.

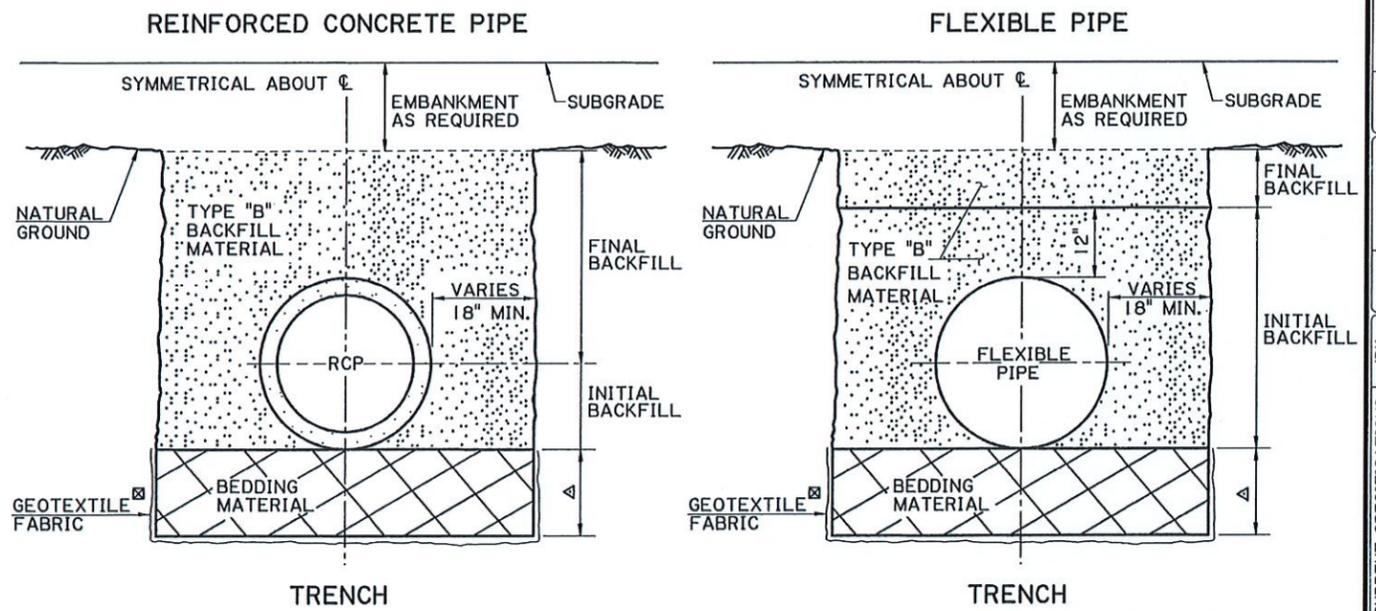
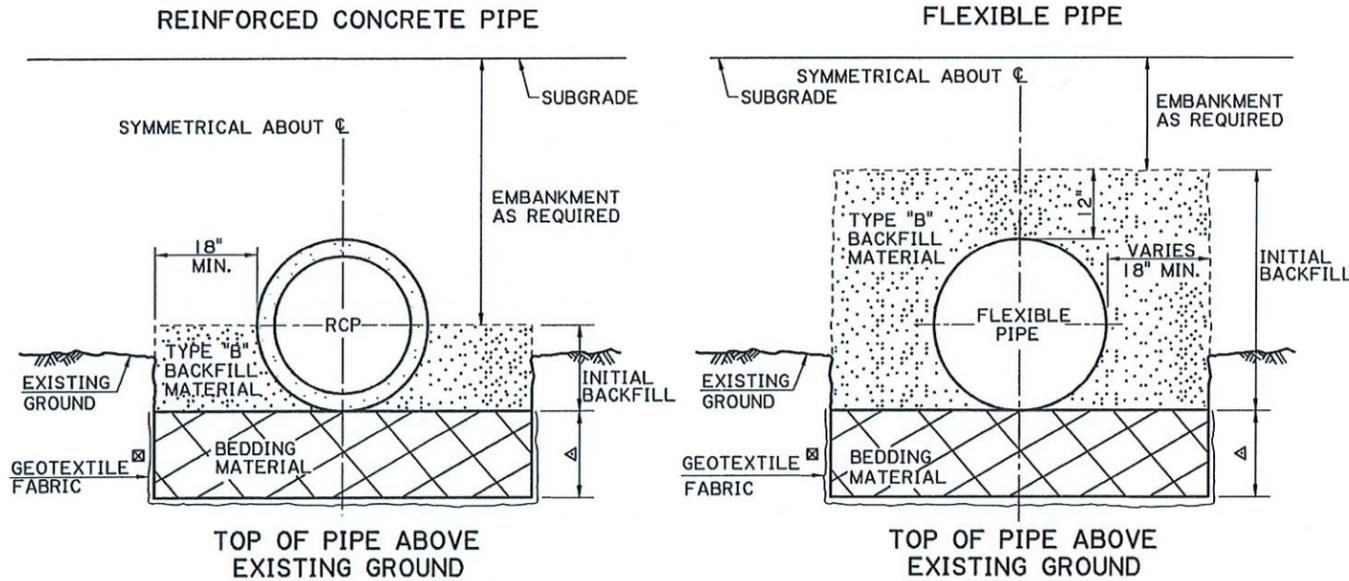


EMBANKMENT INSTALLATION

TRENCH INSTALLATION

① FOR RIGID PAVEMENTS, FLEXIBLE PAVEMENTS OR OTHER AREAS

① FOR RIGID PAVEMENTS, FLEXIBLE PAVEMENTS OR OTHER AREAS

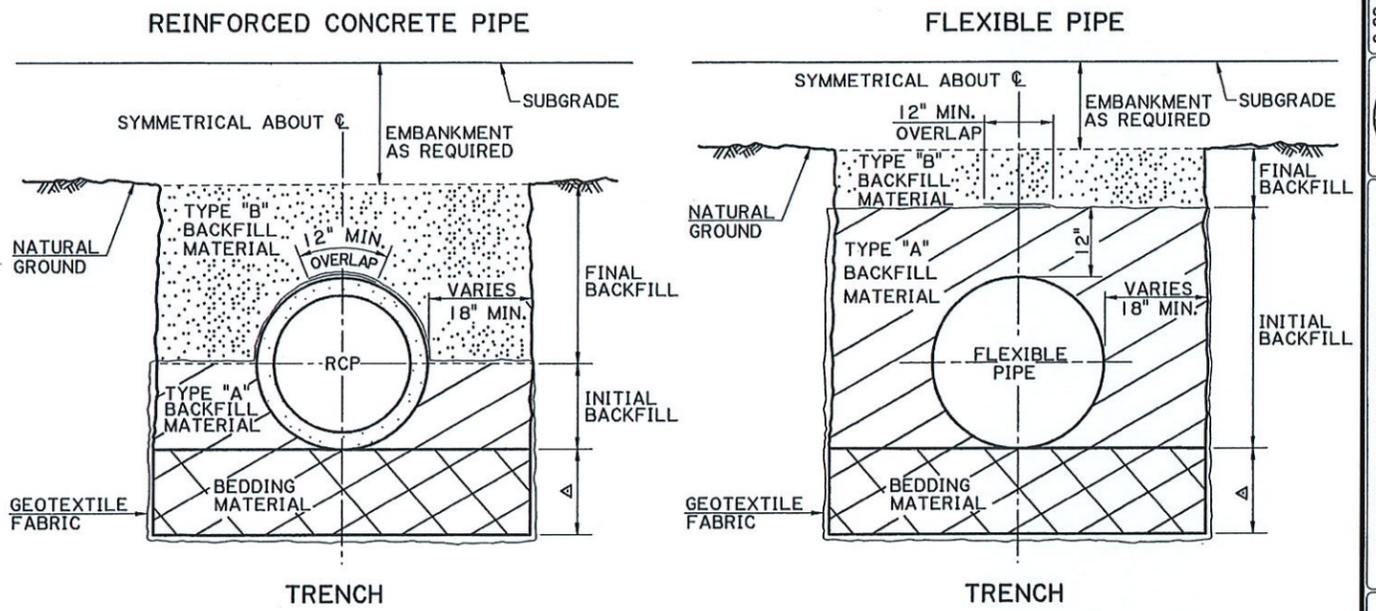
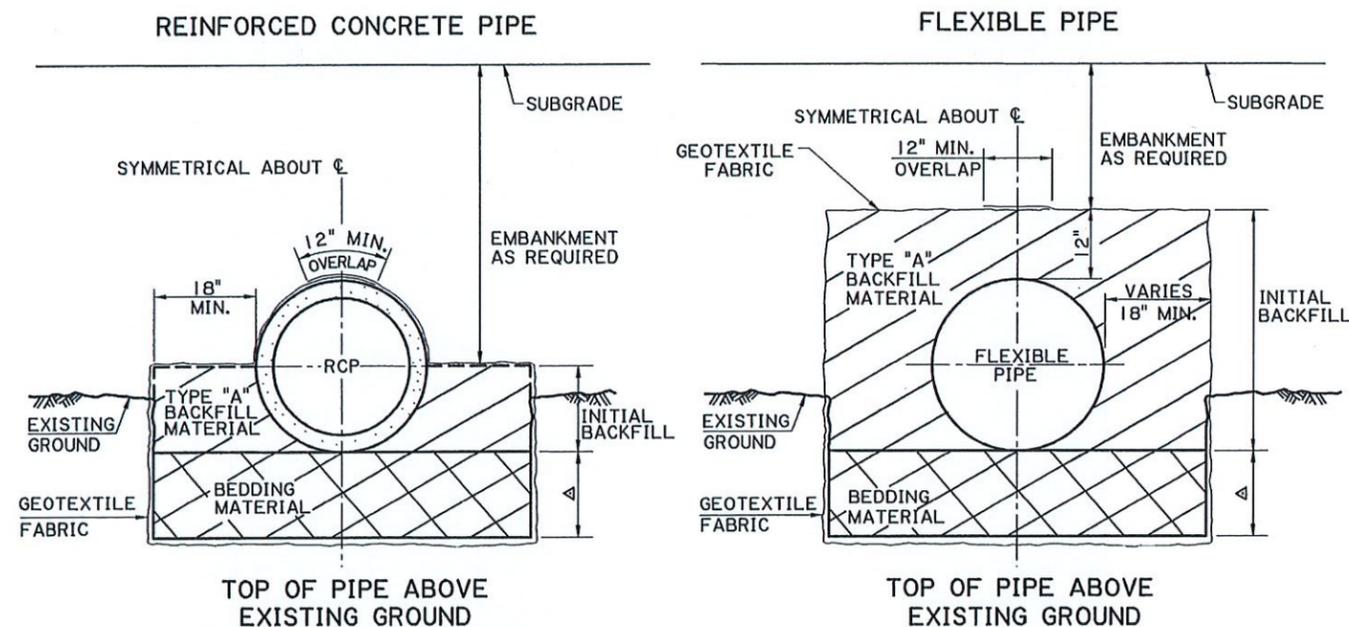


- ① FOR RIGID PAVEMENTS: APPLIES TO ALL PIPE UNDER RIGID PAVEMENT, EXCEPT AS NOTED FOR FLEXIBLE PAVEMENT NOTE ③ BELOW.
- FOR FLEXIBLE PAVEMENTS: APPLIES TO PIPES THAT DO NOT CROSS THE CENTERLINE OF NEW OR EXISTING ROADWAY
- FOR OTHER AREAS: APPLIES TO PIPES IN NONPAVED AREAS OR PAVED AREAS THAT SERVE AS DRIVEWAYS OR SHOULDERS
- ☒ IF DIRECTED BY THE PROJECT ENGINEER, GEOTEXTILE FABRIC WILL BE INSTALLED AROUND THE TYPE "B" BACKFILL AND PAID UNDER THE PAY ITEM FOR GEOTEXTILE FABRIC, SECTION 711 OR 203 OF LA DOTD STANDARD SPECIFICATIONS OR BY CHANGE ORDER.

- ① FOR RIGID PAVEMENTS: APPLIES TO ALL PIPE UNDER RIGID PAVEMENT, EXCEPT AS NOTED FOR FLEXIBLE PAVEMENT NOTE ③ BELOW.
- FOR FLEXIBLE PAVEMENTS: APPLIES TO PIPES THAT DO NOT CROSS THE CENTERLINE OF NEW OR EXISTING ROADWAY
- FOR OTHER AREAS: APPLIES TO PIPES IN NONPAVED AREAS OR PAVED AREAS THAT SERVE AS DRIVEWAYS OR SHOULDERS
- ☒ IF DIRECTED BY THE PROJECT ENGINEER, GEOTEXTILE FABRIC WILL BE INSTALLED AROUND THE TYPE "B" BACKFILL AND PAID UNDER THE PAY ITEM FOR GEOTEXTILE FABRIC, SECTION 711 OR 203 OF LA DOTD STANDARD SPECIFICATIONS OR BY CHANGE ORDER.

FOR FLEXIBLE PAVEMENTS ② ③

FOR FLEXIBLE PAVEMENTS ② ③



- ② APPLIES TO PIPE CROSSING THE CENTERLINE OF NEW OR EXISTING ROADWAYS
- ③ ALSO APPLIES UNDER RIGID PAVEMENTS FOR PIPES CROSSING THE CENTERLINE OF NEW OR EXISTING PAVEMENTS WHEN THE PROJECT IS BID USING A RIGID VS FLEXIBLE ALTERNATE (A + B + C) BID MODEL.

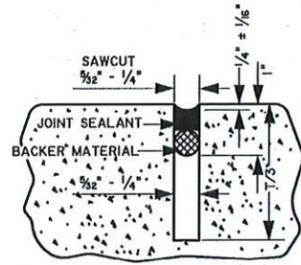
- ② APPLIES TO PIPE CROSSING THE CENTERLINE OF NEW OR EXISTING ROADWAYS
- ③ ALSO APPLIES UNDER RIGID PAVEMENTS FOR PIPES CROSSING THE CENTERLINE OF NEW OR EXISTING PAVEMENTS WHEN THE PROJECT IS BID USING A RIGID VS FLEXIBLE ALTERNATE (A + B + C) BID MODEL.

△ THICKNESS AS SHOWN ON PLANS (6" MIN.) OR AS DIRECTED BY THE PROJECT ENGINEER

△ THICKNESS AS SHOWN ON PLANS (6" MIN.) OR AS DIRECTED BY THE PROJECT ENGINEER

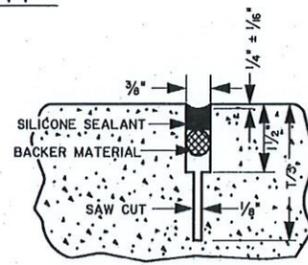
SHEET NUMBER	102
ASCENSION	
DESIGNED	JDK
CHECKED	JDK
DATE	6-8-71
REVISION DESCRIPTION	
8-22-07	REVISION TO COMPLY WITH CURRENT SPECIFICATIONS
1-8-07	ADDED FINAL AND INITIAL BACKFILL
7-11-05	REDRAFTED, REVISED FOR TYPE A & B BACKFILL
DATE	DATE
APPROVED BY	DATE: 8-22-07
CHIEF ENGINEER	
BEDDING AND BACKFILL FOR DRAINAGE STRUCTURES	
HYDRAULICS SECTION	

DETAILS "A-F"



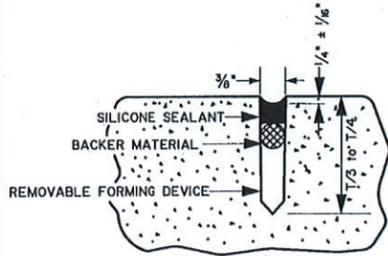
DETAIL "A"

USE THIS DETAIL IN CONJUNCTION WITH TYPE TCJ (SECTION B-B) AND TYPE LJ JOINT (SECTION D-D) AND NOTES ⑥ & ⑦ ON SHEET #1.



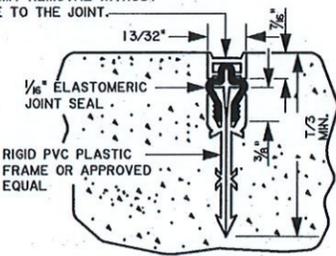
DETAIL "B"

REMOVE CAP AFTER CONCRETE HAS HARDENED SUFFICIENTLY TO PERMIT REMOVAL WITHOUT DAMAGE TO THE JOINT.



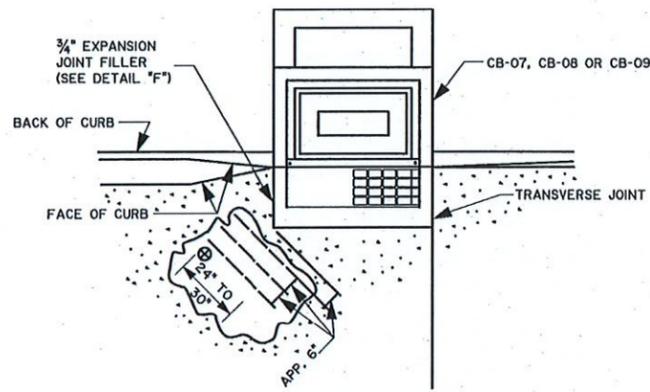
DETAIL "C"

NOT ALLOWED FOR DESIGN SPEEDS GREATER THAN 45 MPH.

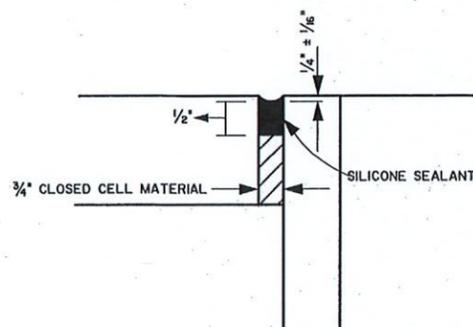


DETAIL "D"

NOT ALLOWED WHEN THE PAVEMENT IS PLACED ON PERMEABLE BASES
NOT ALLOWED FOR DESIGN SPEEDS GREATER THAN 45 MPH.



DETAIL "E"
TRANSVERSE JOINT AT CATCH BASIN



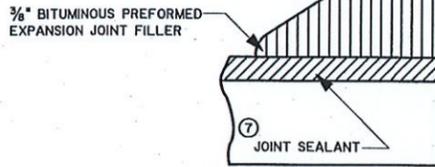
DETAIL "F"

AFTER CATCH BASIN TOP IS POURED, THE TOP OF THE 3/4" JOINT FILLER IS TO BE REMOVED TO THE DEPTH SHOWN PRIOR TO SEALING. THE CURB FACES ADJACENT TO THE BASIN SHALL ALSO BE SEALED. JOINT FACES SHALL BE CLEANED IN ACCORDANCE WITH SECTION 601.

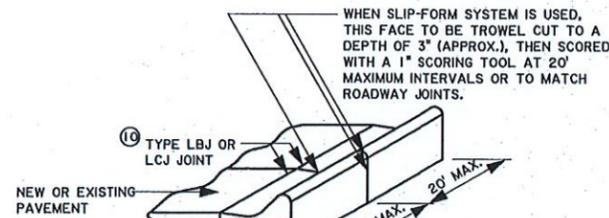
NOTE: SEE STANDARD PLAN DW-01 AND PLANS FOR CURB PLACEMENT DETAILS.

NOTE:

WHEN CURB IS POURED MONOLITHICALLY WITH PAVEMENT, THE BITUMINOUS PREFORMED EXPANSION JOINT FILLER SHALL EXTEND TO THE TOP OF JOINT INSERT. WHEN TRANSVERSE JOINTS ARE CONSTRUCTED BY SAWING, THE INITIAL SAW CUT SHALL EXTEND THROUGH THE CURBED SECTION (CURB AND UNDERLYING PAVEMENT). THE SUBSEQUENT WIDENING CUT FOR THE JOINT SEALANT RESERVOIR SHALL EXTEND INTO THE CURB FOR A DISTANCE NECESSARY TO ENSURE THE SPECIFIED RESERVOIR DEPTH IS BEING MAINTAINED AT THE GUTTER LINE. ALL CURB FACES REGARDLESS OF CURB TYPE SHALL BE SEALED WHEN TRANSVERSE JOINT IS SAWED THROUGH CURB.

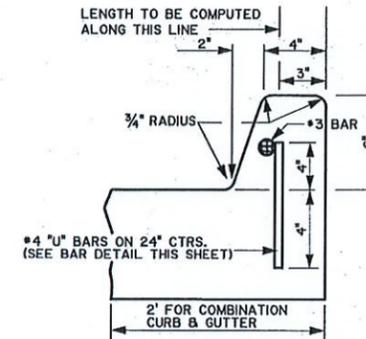


JOINT FILLER DETAIL FOR INTEGRAL CONCRETE CURB (MOUNTABLE OR BARRIER TYPE)

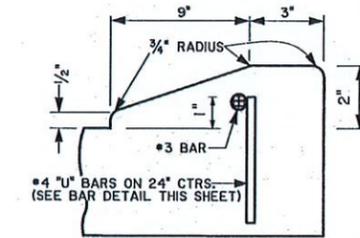


DETAIL SHOWING JOINTS IN CONCRETE CURB AND GUTTER (EXTEND ALL TCJ THROUGH CURB & GUTTER)

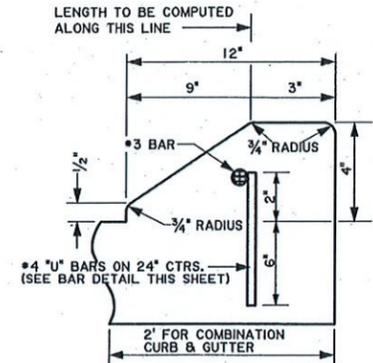
THESE STANDARD PLANS HAVE BEEN PROPERLY EXAMINED BY ME, THE UNDERSIGNED PROFESSIONAL ENGINEER. I HAVE DETERMINED THAT THESE PLANS COMPLY WITH ALL APPLICABLE CODES AND HAVE BEEN PROPERLY ADAPTED TO USE ON THIS PROJECT.



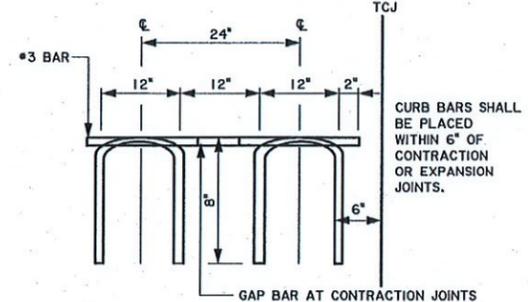
(BARRIER TYPE)



MODIFIED BARRIER OR MOUNTABLE CURB THRU DRIVEWAY



(MOUNTABLE TYPE)



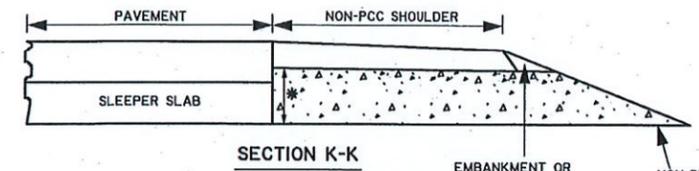
BAR DETAIL

SHOWING DIMENSIONS AND SPACING OF #4 "U" BARS AND LONGITUDINAL BARS FOR CONC. CURB

CURB DETAILS

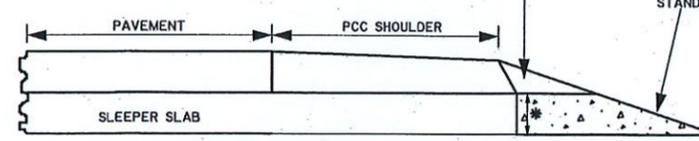
NOTES:

- POUR CURB INTEGRAL WITH PCC PAVEMENT OR GUTTER TO INSURE MONOLITHIC CONSTRUCTION UNLESS OTHERWISE APPROVED BY THE ENGINEER. CURB BARS ARE NOT REQUIRED WHERE CURB IS CONSTRUCTED MONOLITHIC WITH THE PAVEMENT.
- ALL BARS SHOWN SHALL BE DEFORMED REINFORCING STEEL.
- WHEN REPLACING OR ADDING CONCRETE CURB TO EXISTING PAVEMENT, CONNECT THE NEW CONCRETE CURB TO THE PAVEMENT WITH THE DEFORMED REINFORCING STEEL SHOWN BY DRILLING HOLES INTO THE EXISTING PAVEMENT 1/8" LARGER THAN THE BAR DIAMETER. ANCHOR THE BARS WITH AN APPROVED EPOXY RESIN SYSTEM FROM THE DOTD AML. APPLY EPOXY ADHESIVE, COMPLYING WITH AASHTO M235, TYPE V, TO THE SURFACE AREA WHERE THE CONCRETE CURB WILL BE PLACED. INCLUDE ALL COST RELATED TO THE CONSTRUCTION OF THE CURB, INCLUDING THE DRILLED HOLES, DEFORMED REINFORCING BARS, AND EPOXY, IN THE UNIT PRICE FOR THE CURB ITEM.

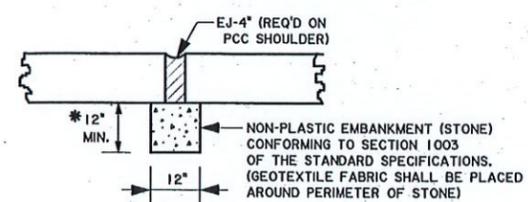


SECTION K-K

* SEE TYPICAL SECTION FOR DEPTH (12" MIN.)



SECTION L-L (WITH CONCRETE SHOULDER)

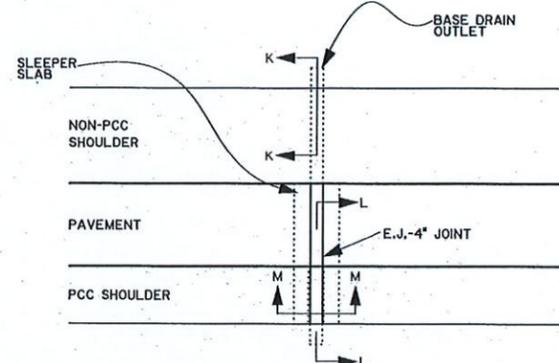


SECTION M-M (SLEEPER SLAB NOT SHOWN)

A BASE DRAIN OUTLET WILL BE REQUIRED AT E.J.-4" JOINTS UNLESS A SHOULDER UNDER DRAIN SYSTEM IS SPECIFIED ON THE PLANS, IN WHICH CASE, THE SHOULDER UNDER DRAIN FOR THE E.J. JOINT SHALL BE CONNECTED TO THE NEAREST STORM SEWER OR DISCHARGED THROUGH A HEADWALL. THE COST FOR THE BASE DRAIN OUTLET FOR THE E.J. JOINT IS TO BE INCLUDED IN THE COST OF THE PAVEMENT.



8/9/2019

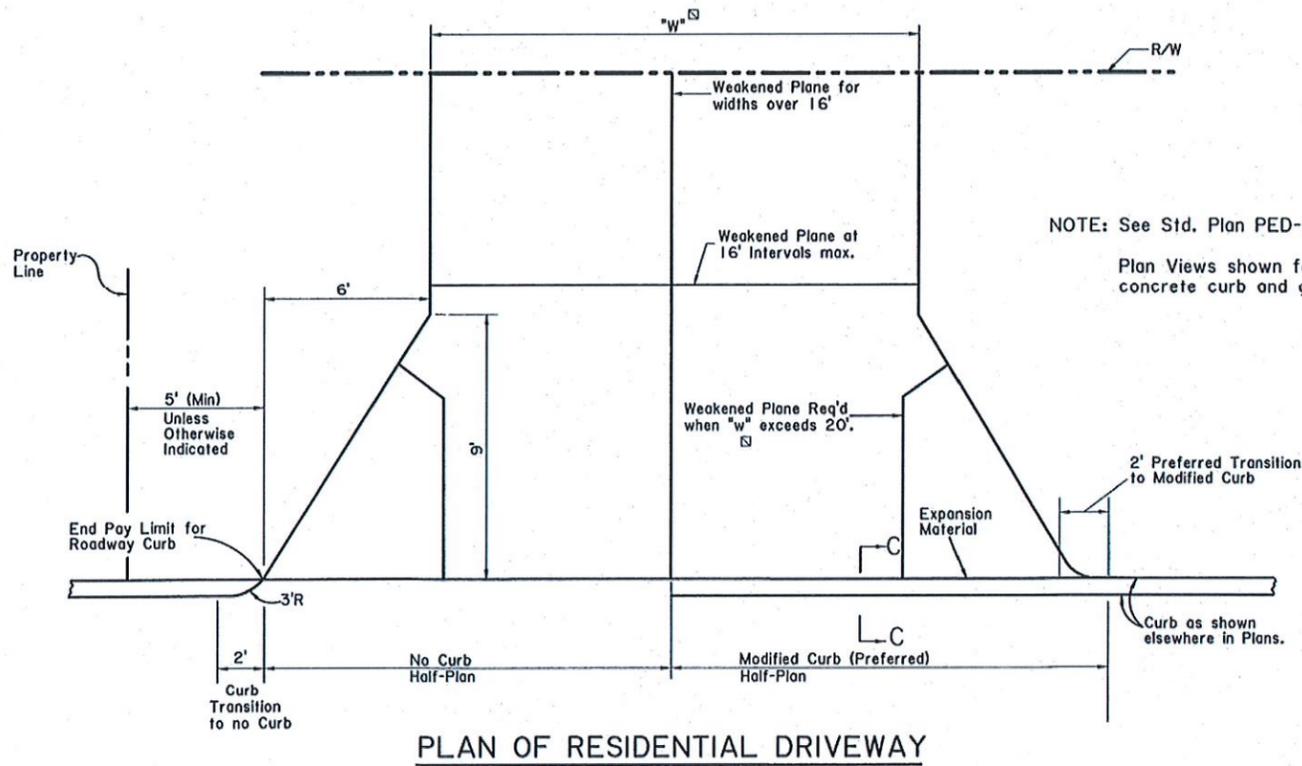


PLAN - BASE DRAIN OUTLET AT 4" E.J. EJ-4" JOINTS

DETAIL "G" - EJ-4" BASE DRAIN OUTLET

NOT TO SCALE

SHEET NUMBER	103
DESIGNED	H. DANG
CHECKED	S. ARDOIN
DETAILER	H. DANG
CHECKED	S. ARDOIN
REVISION OR CHANGE ORDER DESCRIPTION	
DATE	
APPROVED BY	J. P. Williams
CHIEF ENGINEER	
NO.	
DATE	5-11-2017
SERIES NUMBER	3 OF 3
STATE PROJECT	
CONTROL SECTION	
PARISH	ASCENSION
PORTLAND CEMENT CONCRETE PAVEMENT DETAILS	
CP-01 - STANDARD PLAN	
DOTD	
DOTD ROAD DESIGN	



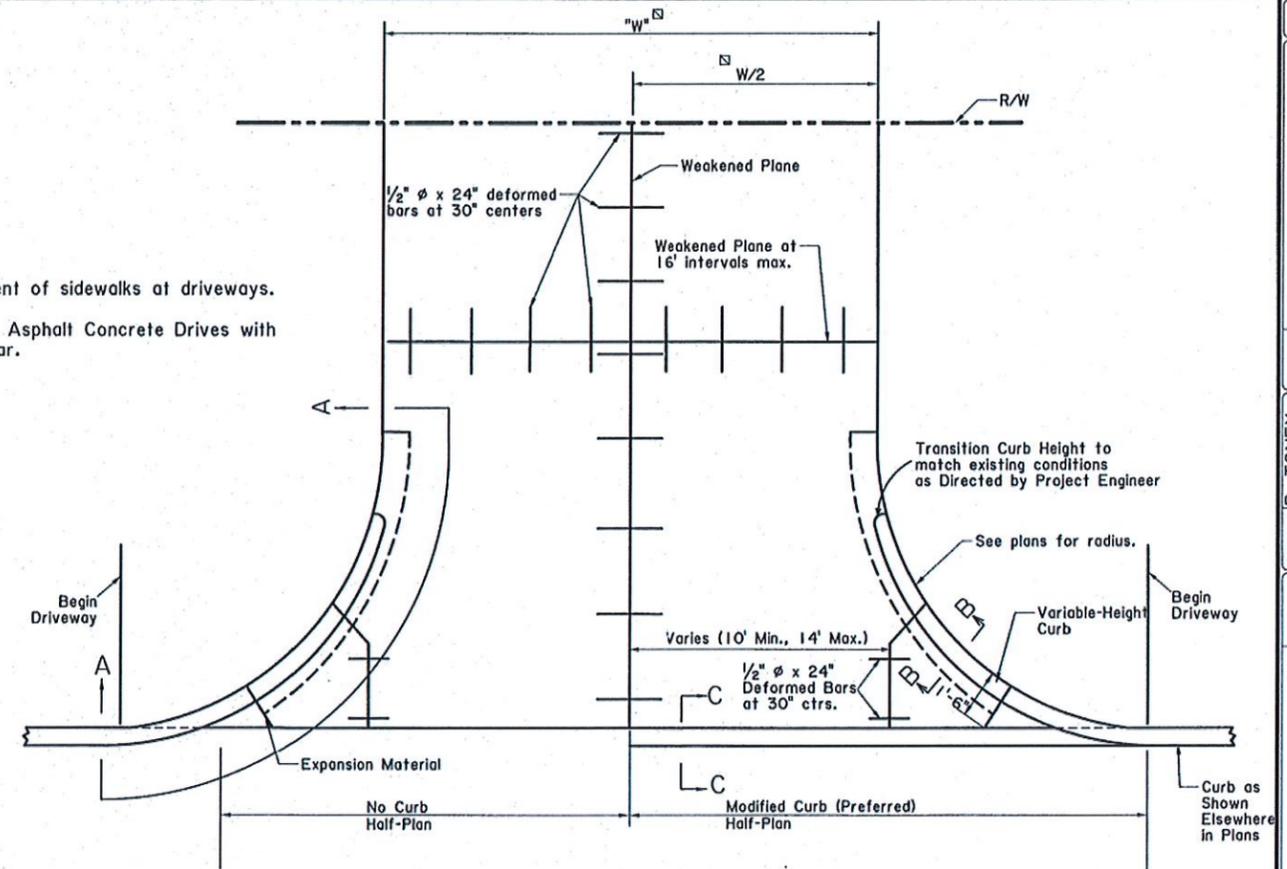
PLAN OF RESIDENTIAL DRIVEWAY

NOTE: Modified Curb to be Paid for as Normal Curb and will be used as shown in the plans or when directed by Project Engineer.

Radii transition shape may be used in lieu of flare.

NOTE: See Std. Plan PED-01 for treatment of sidewalks at driveways.

Plan Views shown for PCC Drives. Asphalt Concrete Drives with concrete curb and gutter are similar.



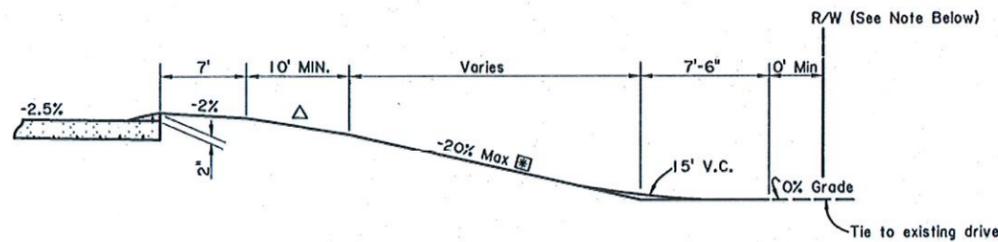
PLAN OF COMMERCIAL DRIVEWAY

NOTE: Modified Curb to be paid for as Normal Curb and will be used as shown in the plans or when directed by Project Engineer.

When Curb is Required along Radii of Driveway, Payment for Toe Wall and Curb will be included in the price for Driveway items.

When Curb is not Required along Radii, Transition Curb as shown on Residential Driveway.

W = Width as per plans

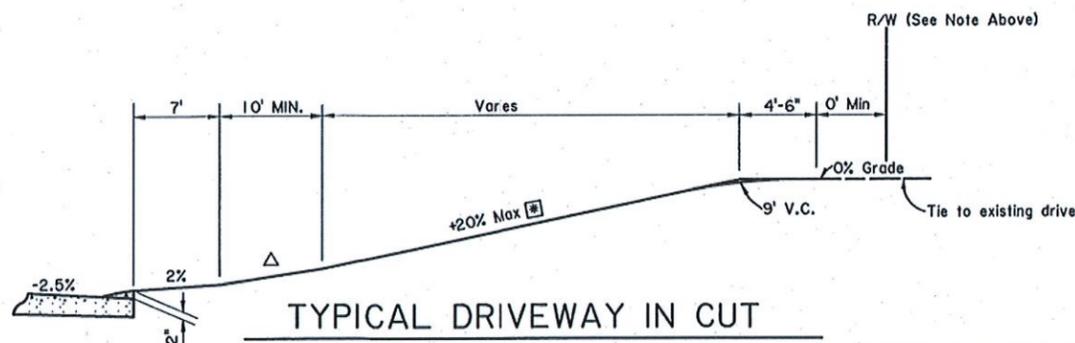


TYPICAL DRIVEWAY IN FILL

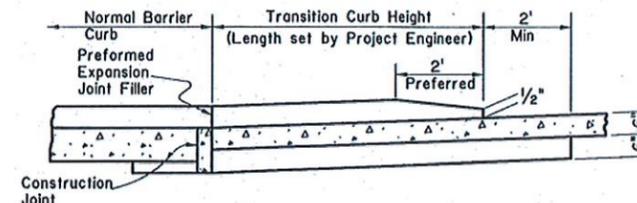
NOTE: Driveway Grades shown are Maximum. If R/W is not sufficient to construct Driveway, R/W Agreement or Construction Servitude should be secured to permit Construction of Driveway as shown.

25% Max Grade may be used in Special Cases, provided that Vertical Curve Lengths are increased accordingly.

Maximum change in grade shall be 10% for Crests and 9% for Sags for any combination of grades without Vertical Curves. These Maximum Grade changes should be at least 10' apart.

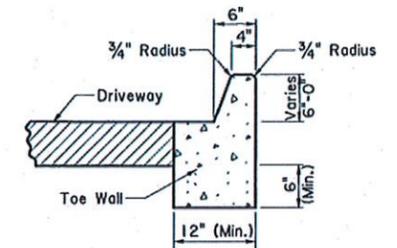


TYPICAL DRIVEWAY IN CUT



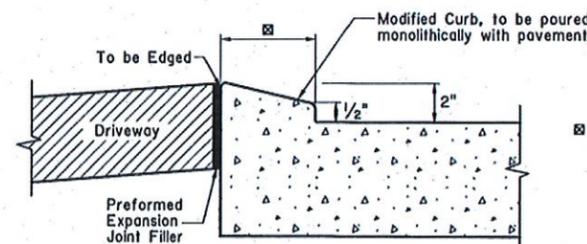
SECTION A-A

(Weakened Plane not shown.)



SECTION B-B

NOTE: For PCC Driveway, Curb, Toe Wall & Driveway to be poured monolithically.



SECTION C-C

NOTE: See Std. Plan CP-01 for Curb construction

6\"/>



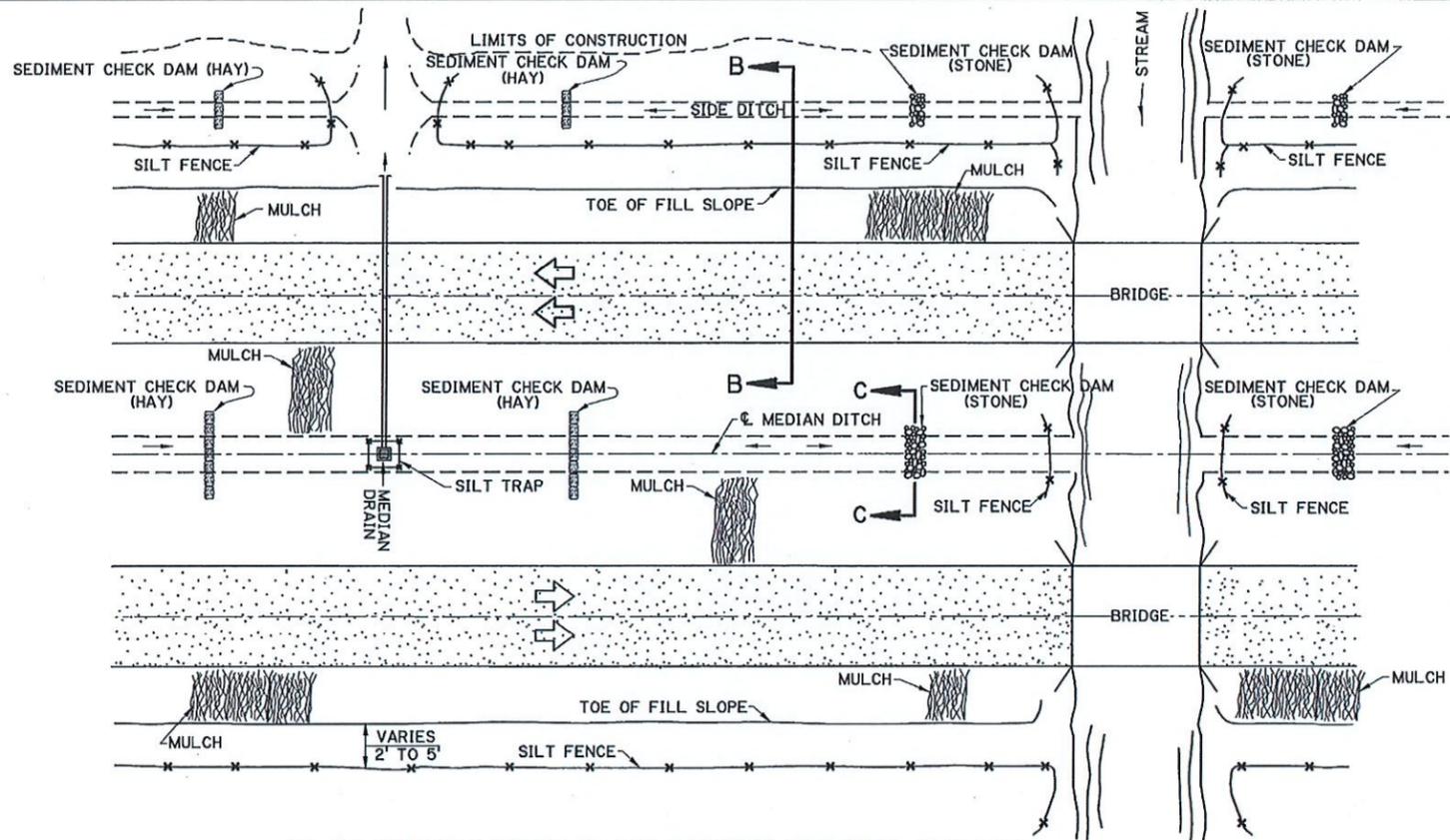
8/9/2019



11/7/2017

THESE STANDARD PLANS HAVE BEEN PROPERLY EXAMINED BY ME, THE UNDERSIGNED PROFESSIONAL ENGINEER. I HAVE DETERMINED THAT THESE PLANS COMPLY WITH ALL APPLICABLE CODES AND HAVE BEEN PROPERLY ADAPTED TO USE ON THIS PROJECT.

SHEET NUMBER	104
DESIGNED BY	P. TONEY
CHECKED BY	D. SMITH
DATE	11/7/17
REVISION DESCRIPTION	
NO.	
DATE	
APPROVED BY	James P. Williams
CHIEF ENGINEER	
PARISH	ASCENSION
CONTROL SECTION	
STATE PROJECT	
SERIES NUMBER	1 of 1
BY	
DATE	
REVISION DESCRIPTION	
NO.	
DATE	
APPROVED BY	
CHIEF ENGINEER	
DRIVEWAYS ON CURBED ROADWAYS STANDARD PLAN DW-01	
ROAD DESIGN	

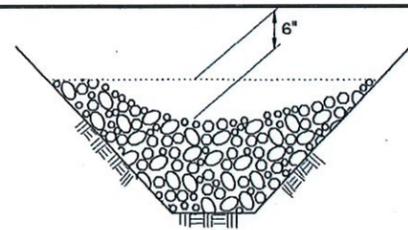


PLAN SHOWING TYPICAL TEMPORARY EROSION CONTROL

MULCHES

MULCHES ARE THE APPLICATION OF MATS OF MATERIAL PLACED ON THE SOIL SURFACE TO PREVENT EROSION BY PROTECTING THE SOIL SURFACE FROM RAINDROP IMPACT AND TO REDUCE THE VELOCITY OF OVERLAND FLOW. MULCHES CAN BE ORGANIC OR SYNTHETIC. MULCHES SHALL BE IN ACCORDANCE WITH PROJECT SPECIFICATIONS FOR TEMPORARY EROSION CONTROL. A FEW GUIDELINES FOR THE USE OF MULCHES ARE:

1. USE ON CUT AND EMBANKMENT SLOPES WHICH HAVE NOT BEEN COMPLETED TO PLAN GRADE OR WHERE THE WEATHER OR SOIL CONDITIONS WILL NOT PERMIT COMPLETING THEM WITHIN A REASONABLE TIME
2. USE ON CLEARED, GRUBBED, AND SCALPED AREAS WHERE SOIL EROSION IS LIKELY TO OCCUR
3. USE WITH TEMPORARY SEEDING



TEMPORARY SEDIMENT CHECK DAM (STONE)

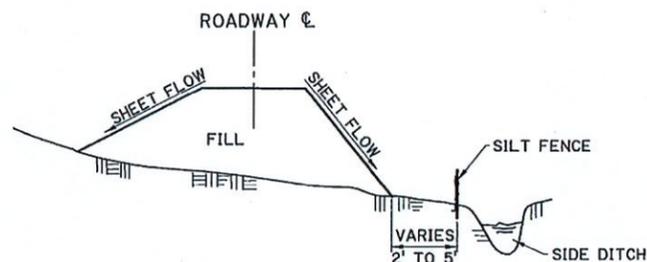
PAY ITEM: TEMPORARY SEDIMENT CHECK DAM (STONE)

NOTES:

A STONE CHECK DAM IS A SMALL TEMPORARY DAM CONSTRUCTED ACROSS A SWALE OR DRAINAGE DITCH. THE PURPOSE OF THIS MEASURE IS TO REDUCE THE VELOCITY OF CONCENTRATED STORM WATER FLOWS, THEREBY REDUCING EROSION OF THE SWALE OR DITCH. THE STONE CHECK DAM WILL TRAP SMALL AMOUNTS OF SEDIMENTS GENERATED IN THE DITCH ITSELF, HOWEVER IT SHOULD NOT BE USED AS A SEDIMENT TRAPPING DEVICE. A FEW BASIC DESIGN GUIDELINES FOR THE USE OF STONE CHECK DAMS ARE:

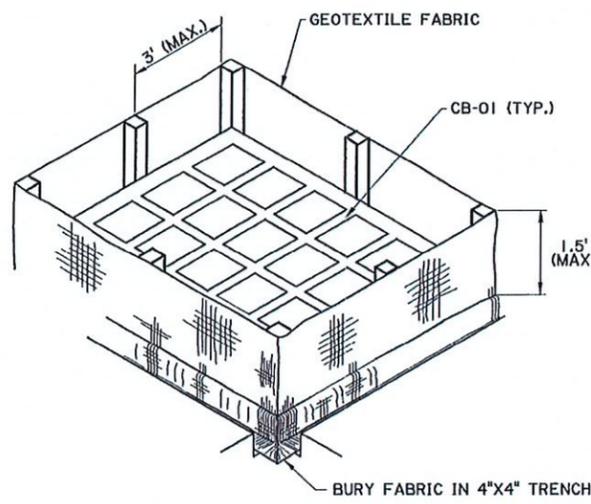
1. USE IN SMALL OPEN CHANNELS WHICH DRAIN 10 ACRES OR LESS
2. DO NOT USE IN A LIVE STREAM
3. USE IN A TEMPORARY DITCH OR SWALE WHICH, BECAUSE OF THEIR SHORT LENGTH OF SERVICE, CANNOT RECEIVE A NON-ERODIBLE LINING
4. USE IN PERMANENT DITCHES OR SWALES WHICH WILL NOT RECEIVE A PERMANENT LINING FOR AN EXTENDED PERIOD OF TIME
5. USE IN TEMPORARY OR PERMANENT DITCHES OR SWALES WHICH NEED PROTECTION DURING THE ESTABLISHMENT OF GRASS LININGS
6. FOR STONE SPECIFICATIONS, SEE PROJECT SPECIFICATIONS FOR RIPRAP, (CLASS 2 LB)

THESE STANDARD PLANS HAVE BEEN PROPERLY EXAMINED BY ME, THE UNDERSIGNED PROFESSIONAL ENGINEER. I HAVE DETERMINED THAT THESE PLANS COMPLY WITH ALL APPLICABLE CODES AND HAVE BEEN PROPERLY ADAPTED TO USE ON THIS PROJECT.



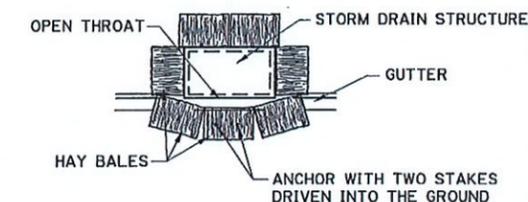
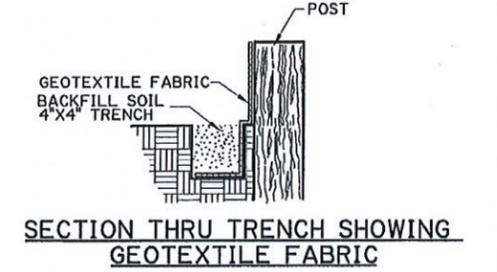
TEMPORARY SILT FENCE APPLICATION

(FOR CONSTRUCTION DETAILS AND SPECIFICATIONS SEE SHEET 2 OF 2.)



ISOMETRIC VIEW SHOWING GEOTEXTILE FABRIC

(BACKFILL SOIL NOT SHOWN)

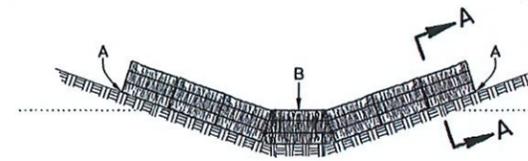


PAY ITEM: TEMPORARY HAY OR STRAW BALES

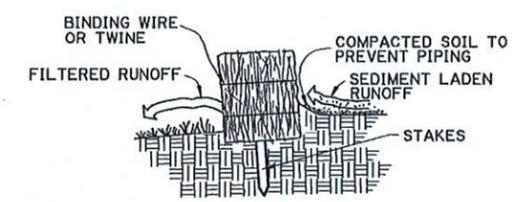
TEMPORARY INLET SILT TRAP

THE TEMPORARY DROP INLET SILT TRAP IS TO BE USED FOR SMALL DRAINAGE AREAS (LESS THAN 1 ACRE) WHERE THE STORM DRAIN IS FUNCTIONAL BEFORE THE AREA IS STABILIZED. THE TRAP CAN BE EITHER GEOTEXTILE FABRIC OR HAY BALES.

1. THE GEOTEXTILE FABRIC SHALL CONFORM TO PROJECT SPECIFICATIONS FOR GEOTEXTILE FABRIC (CLASS G).
2. WOODEN STAKES SUPPORTING THE FABRIC SHALL BE 2" X 2" OR 2" X 4" WITH A MINIMUM LENGTH OF 3 FEET. THE STAKES SHALL BE SPACED AROUND THE INLET AT A MAXIMUM SPACING OF 3 FEET.
3. THE HEIGHT OF THE FABRIC ABOVE THE INLET SHALL BE LIMITED TO 1.5' AND THE BOTTOM OF THE FABRIC SHALL BE BURIED IN A TRENCH APPROXIMATELY 4" WIDE BY 4" DEEP. THE FABRIC SHALL BE STAPLED TO THE POST WITH 1/2" STAPLES.
4. THE TRAP SHOULD BE INSPECTED REGULARLY AND AFTER EACH STORM. THE SEDIMENT SHOULD BE REMOVED AND EACH STAKE SHOULD BE FIRMLY IN THE GROUND.
5. HAY BALES SHALL BE PLACED SO THAT THE BINDING WIRE OR TWINE IS NOT IN CONTACT WITH THE GROUND.



POINTS A SHOULD BE HIGHER THAN POINT B.



TEMPORARY SEDIMENT CHECK DAM (HAY)

PAY ITEM: TEMPORARY SEDIMENT CHECK DAM (HAY)

NOTES:

A HAY BALE BARRIER IS A TEMPORARY SEDIMENT BARRIER CONSISTING OF A ROW OF ENTRENCHED AND ANCHORED BALES OF STRAW OR HAY. THE HAY BALE BARRIER IS ALSO USED AS A CHECK DAM TO REDUCE THE VELOCITY IN SMALL DITCHES OR SWALES. THE HAY BALES SHALL BE IN ACCORDANCE WITH PROJECT SPECIFICATIONS FOR TEMPORARY EROSION CONTROL. A FEW BASIC DESIGN GUIDELINES FOR THE USE OF A HAY BALE BARRIER ARE:

1. USE WHERE EROSION WOULD OCCUR IN THE FORM OF SHEET AND RILL EROSION
2. USE IN MINOR SWALES OR DITCHES WHERE THE MAXIMUM DRAINAGE AREA IS 2 ACRES
3. ONLY USE WHERE THE EFFECTIVENESS IS REQUIRED FOR LESS THAN 3 MONTHS
4. DO NOT USE IN LIVE STREAMS OR IN SWALES OR DITCHES WHERE THERE IS A POSSIBILITY OF A WASHOUT



SHEET NUMBER	105
ASCESSION	ASCENSION
PARISH	FEDERAL PROJECT STATE PROJECT
DESIGNED BY	JCM
CHECKED BY	KAJ
DATE	1-14-04
SHEET	1 OF 2
REVISIONS	GENERAL REVISIONS
DATE	10-1-08
APPROVED BY	W. H. Temp
CHEF ENGINEER	
TEMPORARY EROSION CONTROL DETAILS	EC-01
HYDRAULICS SECTION	

GENERAL PROVISIONS

- All temporary traffic control (TTC) devices used shall be in accordance with the Louisiana Standard Specifications for Roads and Bridges, the MUTCD, and shall meet the NCHRP Report 350 or MASH requirements for Test Level 3 devices where applicable.
- Materials used for TTC shall be in accordance with the Louisiana Standard Specifications for Roads and Bridges and, when applicable, the LADOTD AML.
- Placement of TTC devices shall not commence without the approval of the Engineer and until work is about to begin, unless they are covered.
- No lane closures, lane shifts, diversions or detours shall occur without the approval of the Engineer.
- Responsibility is hereby placed upon the contractor for the installation, maintenance and operation of all TTC devices called for in these plans or required by the Engineer for the protection of the traveling public as well as all LADOTD and construction personnel.
- The contractor shall also be responsible for the maintenance of all permanent signs, pavement markings, and traffic signals left in place as essential to the safe movement and guidance of traffic within the project limits unless noted in the plans.
- The DTOE shall serve as a technical advisor to the Engineer for all traffic control matters.
- The Chief Construction Engineer or his appointed designee shall approve all signs and situations not addressed in the plans based on the recommendations of the Project Engineer and the DTOE. All changes shall be noted in all project traffic control diaries.
- The Chief Construction Engineer or his appointed designee shall approve all design speeds of diversions or shifts, if it differs from design plans, based on the recommendations of the Project Engineer and the DTOE.
- All temporary traffic control plans shall comply with the Transportation Management Plan.
- Any additional signs shown in the MUTCD and required by the Engineer shall be installed under Item 713-01-00100.
- Neither work activity nor storage of equipment, vehicles, TMAs, or materials shall occur within the buffer space.
- When a work area has been established on one side of the roadway only, there shall be no conflicting operations or parking on the opposite shoulder within 500 feet of the work area.
- A lighting plan shall be submitted to the Engineer 30 days prior to night work for approval. (See section 105.20 of the Louisiana Standard Specifications for Roads and Bridges.)
- Parking of vehicles or unattended equipment or storage of materials, within the clear zone shall not be permitted unless protected by guardrail or barriers. If the clear zone is not defined on the plan sheets, the Engineer shall verify.
- Immediately upon removal of existing guardrail, the contractor shall install and maintain an NCHRP Report 350 or MASH approved device to protect the blunt end of the bridge or column until new guardrail is installed. After removal of the existing guardrail, new guardrail should be installed within seven (7) days. On non-NHS routes with shoulders less than 8 feet wide: If an NCHRP 350 Report Test Level 3 or MASH device is required but the field conditions of the roadway cannot support a Test Level 3 device, then a Test Level 2 device can be substituted in its place upon approval by the Engineer. If utilized, a TMA is allowed for a maximum of 72 hours.
- All costs associated with crash devices are to be included in Item 713-01-00100.
- Sight distance should be considered when placing traffic control devices.
- On all mainline Interstates, a minimum of 1.5 feet of paved shoulder on the left and right side shall be maintained at all times.

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- On Interstates, a minimum of 11 foot lanes shall be maintained. On all other roadways, a 10 foot minimum travel lane should be maintained where practical.
- TTC Standards are not drawn to scale.
- The contractor shall develop an internal traffic control plan approved by the Engineer prior to each phase.
- Truck restrictions such as (but not limited to) restricting lanes, oversize loads or times of travel, may be required for narrow lanes or other field conditions.

PAVEMENT MARKINGS (see AML)

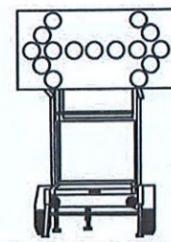
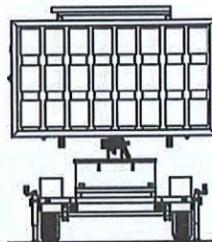
- All pavement markings within the limits of the project or adjacent to the project limits that are in conflict with the project signing or the required traffic movements shall be removed from the pavement by blast cleaning or grinding. (Existing striping shall not be painted over with black paint or covered with tape.)
- If special pavement markings are needed, they shall be reflectorized, removable and accompanied by the proper signage.
- Temporary Raised Pavement Markers may be added to supplement temporary striping in areas of transition, in tapers, in diversions and in other areas of need as shown in the plans or as directed by the Engineer.
- Materials and placement of temporary pavement markings shall conform to Section 713 of the Louisiana Standard Specifications for Roads and Bridges. If no pay item exists for temporary markings, they shall be installed under item 713-01-00100.
- Temporary markings installed in the permanent configuration shall comply with LADOTD pavement marking standard plans, MUTCD and/or the permanent striping plans.

PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)

- PCMS shall be used on all Interstate Highways. PCMS shall be used on all other roadways (where space is available) with an ADT greater than 20,000.
- When used in advance of a lane closure or a lane shift, the PCMS should be placed on the right hand side of the road a minimum distance of 2 miles in advance of the taper for interstates and to be determined by the Engineer on other highways.
- For interstates and multi-lane highways, if vehicles are queuing beyond the 2 mile PCMS, an additional PCMS should be placed on the right hand side of the road approximately 5 miles in advance of the taper or at the end of the queue, whichever is greater.
- PCMS messages shall be approved by the DTOE. Messages shall be no more than 3 lines and 2 screens.
- Messages shall display only traffic operational, regulatory, warning, and guidance information. PCMS messages shall not display advertising or safety messages. Messages should only convey information concerning the problem/situation, location, and recommended driver action.
- PCMS should be placed as far from the traveled lane as possible. They shall be shielded by guardrail or barriers. If this is not possible they shall be delineated with a min. 3 drum taper spaced at 20ft with a 4th drum alongside the PCMS.
- If the PCMS encroaches on the improved shoulder then the contractor shall install a shoulder closure.
- When the PCMS is not displaying a work zone appropriate message pertaining to the ongoing construction project it shall be shielded by guard rail or barriers, or removed from the clear zone.



8/9/2009



6-27-18

ALL TTC STANDARDS SHOW MINIMUM CONSTRUCTION SIGNING.
ALL SITUATIONS SHALL BE REVIEWED AND/OR DESIGNED BY THE ENGINEER.
CONTRACTORS ARE RESPONSIBLE FOR COMPLYING WITH ALL TTC STANDARDS.

SPEED LIMITS

- The Engineer may approve a 10 mph drop in the speed limit for posted speeds of 45 mph or greater and for any construction, maintenance or utility operation that requires one or more of the following:
 - (A) The condition of the traveled way is degraded due to milled surfaces or uneven travel lane lines greater than 1.5 inches.
 - (B) Work is in progress in the immediate vicinity of the travel way requiring lane closures or lane width reductions less than 11 feet.
 - (C) Workers present on the shoulder within 2 feet of the edge of the traveled way without barrier protection.
- The reduced speed zone shall only apply to those portions of the project limits affected. The Engineer may allow SPEED LIMIT WHEN FLASHING signs to supplement reduced speed zones.
- If the speed limit is reduced, speed limit signs shall be placed:
 - (A) beyond major intersections;
 - (B) at one mile intervals in rural areas;
 - (C) at half mile intervals in urban areas.
- At the end of the reduced speed zone, a speed limit sign displaying the original speed limit prior to construction shall be installed.
- For all other speed limit reductions not listed above, the Project Engineer and the DTOE shall recommend the speed reduction to the Chief Construction Engineer or his appointed designee for approval.
- If the speed limit is reduced more than 10 mph, placement of the signs shall be re-evaluated according to the MUTCD.

FLASHING ARROW BOARDS

- All Flashing Arrow Boards shall be 4 feet by 8 feet and Type C.
- Flashing Arrow Boards should be placed on the shoulder. When there is no shoulder or median area, the arrow board shall be placed within the closed lane behind the channelizing devices and as close to the beginning of the taper as practical.
- Flashing arrow boards shall be delineated with retroreflective TTC devices.
- At no time shall the arrow board encroach in the traveled way. When Flashing Arrow Board signs are not being used, they shall be shielded by guard rail or barriers, or removed.
- Arrow boards shall only be used for lane reduction tapers and shall not be used for lane shifts.

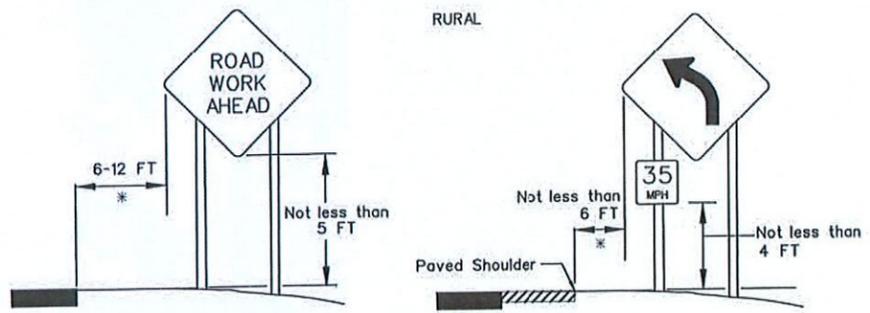
ABBREVIATIONS

- AASHTO American Association of State Highway and Transportation Officials
- ADT Average Daily Traffic
- AGCI Associated General Contractors of America
- AML Approved Materials List
- ANSI American National Standards Institute
- ATSSA American Traffic Safety Services Association
- B.O.P. Beginning of Project
- DTOE District Traffic Operations Engineer
- E.O.P. End of Project
- LADOTD Louisiana Department of Transportation and Development
- MASH AASHTO Manual for Assessing Safety Hardware
- MUTCD Manual on Uniform Traffic Control Devices
- NCHRP National Cooperative Highway Research Program
- NHS National Highway System
- PCMS Portable Changeable Message Sign
- TMA Truck Mounted Attenuator
- TMC Traffic Management Center
- TTC Temporary Traffic Control
- TTC Standards .. Temporary Traffic Control Standard Plans

SHEET NUMBER	107
DESIGNED BY	G. LEBLANC
CHECKED BY	J. COLVIN
DRAWN BY	C. FAKOURI
CHECKED BY	G. LEBLANC
SERIES NUMBER	
DATE	7/2/18
REVISION OR CHANGE ORDER DESCRIPTION	
NO.	
DATE	
APPROVED BY	
CHIEF ENGINEER	
STATE	ASCENSION
PARISH	
GENERAL SECTION	
STATE PROJECT	
TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET TTC-00 (A)	

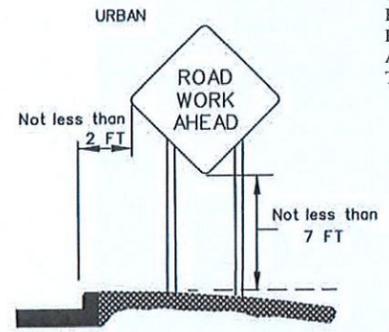
SIGNS

- All signs used for temporary traffic control shall follow the plans, the LADOTD TTC Standards and the MUTCD.
- Signs shown in the TTC illustrations are typical and may vary with each specific condition.
- One Type B High Intensity light shall be used to supplement the first sign (or pair of signs) that gives warning about a lane closure during nighttime operations (See AML).
- Mesh rollup signs shall not be allowed on any project.
- Contractor shall use caution not to damage existing signs which remain in place. Any LADOTD signs damaged by work operations shall be replaced by the contractor under item 713-01-00100.
- All signs (permanent and temporary) shall be removed or completely covered with a strong, lightweight, opaque material when no longer applicable. (Burlap is not an acceptable material to cover signs).
- At no time shall signs warning against a particular operation be left in place once the operation has been completed or where the condition has been removed.
- Warning signs used for temporary traffic controls shall meet the following guidelines unless otherwise noted in the plans:
 - (A) size shall be 48 inches by 48 inches.
 - (B) see the Louisiana Standard Specifications for Roads and Bridges and the AML for sheeting information.
 - (C) lateral distance of signs shall be a minimum of 6 feet from the edge of shoulder or edge of pavement if no shoulder exists and 2 feet from the back of curb in urban areas (see diagram).
- When portable sign frames are not in use, they shall be moved to an area inaccessible to traffic and not visible to the driver.
- Left side mounted signs will not be required for roadways with a center left turn lane and for undivided roadways.
- Vinyl rollup signs may be used if work zone is in place for 12 hours or less, there are no more than 2 lanes in each direction and if signs meet all size, color, retroreflectivity and NCHRP 350 Report or MASH requirements.
- All signs shall be visible to the drivers (i.e. no obstructions such as on street parking or other traffic control devices shall block the sign).
- On divided highways, signs shall be placed on the right and the left as shown on the TTC standards.
- 1 foot portable sign stands may be used if the work zone is in place for 14 hours or less and there are no more than 2 lanes in each direction.
- Sign posts:
 - Signs measuring 10 square feet or less shall be mounted on 1 rigid post
 - Signs over 10 square feet shall be mounted on 2 rigid posts
 - Signs over 20 square feet shall be mounted on at least 3 rigid posts
- Rigid sign supports shall be driven to a minimum depth of 3 feet. (If splicing is required, see Allowable Lap Splice U-channel Post.)
- For sign height, see the Rural and Urban diagrams:



* If lateral distance is not practical, the sign may be placed no less than 2 feet.

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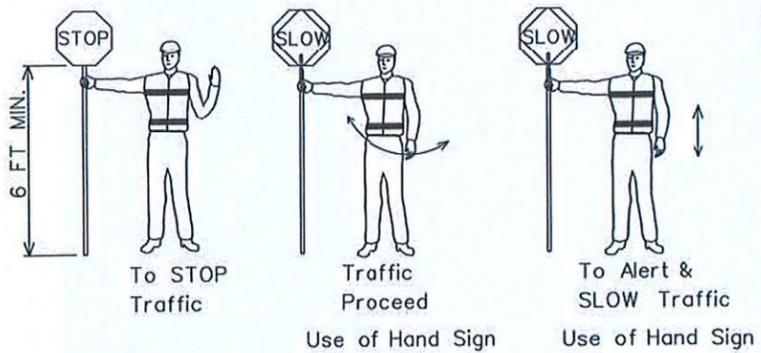
STATE OF LOUISIANA
 DENNIS M. HYNEL JR.
 LICENSE NO. 58172
 PROFESSIONAL ENGINEER
 IN
 CIVIL ENGINEERING
 8/9/2018

LANE CLOSURES

- All proposed lane, road or shoulder closures shall be reviewed by the DTOE and approved by the Engineer.
- Two lane, two-way highways shall have a maximum work area of two miles; all other roadways shall have a four mile maximum work area.
- A queue analysis shall be performed prior to approval of lane closures on all Interstates according to Section 6A.1 of the Traffic Engineering Manual.
- Closure plans and times shall be turned in to the Engineer for review according to the following:
 - (A) 5 working days minimum if traffic control plan has been approved or is contained in the plans.
 - (B) 10 working days minimum and a traffic control plan must be submitted for lane closures not addressed in the plans.
- Weekly updates to the DTOE, Project Engineer, the LADOTD TMC operator and the regional TMC operator (if applicable) will be required for all ongoing lane closures to update the closure status.
- Daily updates to the DTOE, Project Engineer and TMC operator (if applicable) will be required for all projects where active closures are in place.

FLAGGERS

- All flaggers shall be qualified.
- The contractor shall be responsible for training or assuring that all flaggers are qualified to perform flagging duties.
- A Qualified Flagger is one that has completed courses such as those offered by ATSSA or other courses approved by the LADOTD Work Zone Task Force. The contractor shall be responsible for getting the flagger course approved.
- When utilized, a flagger shall use a minimum 18 inch octagonal shape sign on a minimum 6 foot stop/slow paddle and wear ANSI Class 2 Lime Green vest during day time operations and ANSI Class 3 Lime Green ensemble during night operations.
- In all flagging operations, the flagger must be visible from the flagger advance warning sign.
- Flaggers shall not be used on the Interstate.



STATE OF LOUISIANA
 GARY N. LEBLANC
 REG. NO. 23220
 REGISTERED PROFESSIONAL ENGINEER
 IN
 CIVIL ENGINEERING
 6-27-18

PEDESTRIAN CONSIDERATIONS

- If the TTC zone affects the movement of pedestrians, adequate pedestrian access and walkways shall be provided either through the TTC zone or a designated alternate route.
- Pedestrians should be provided with a convenient and accessible path that replicates as nearly as practical the most desirable characteristics of the existing sidewalk(s) or footpath(s).
- Advance notification of sidewalk closures shall be provided by the maintaining agency.

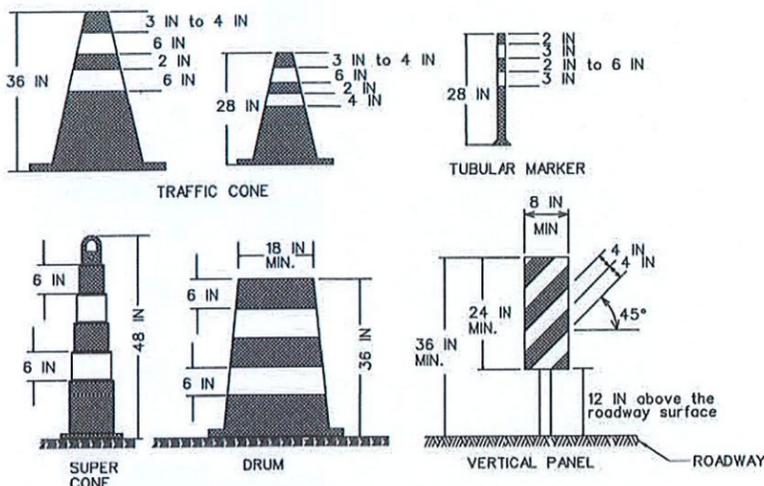
REFERENCES

- The contractor shall be responsible for understanding all rules and requirements in the current edition of the following documents:
 - 1) Louisiana Standard Specifications for Roads and Bridges. <http://www.dotd.la.gov/highways/specifications/>
 - 2) Manual on Uniform Traffic Control Devices for Streets and Highways (MUTCD). <http://mutcd.fhwa.dot.gov/>
 - 3) LADOTD Approved Materials List (AML) Manual. http://wwwsp.dotd.la.gov/Inside_LaDOTD/Divisions/Engineering/Materials_Lab/Pages/Menu_QPL.aspx
 - 4) LADOTD Traffic Engineering Manual http://wwwsp.dotd.la.gov/Inside_LaDOTD/Divisions/Engineering/Traffic_Engineering/Misc%20Documents/Traffic%20Engineering%20Manual.pdf
 - 5) National Cooperative Highway Research Program (NCHRP) Report 350: "Guidelines for Work Zones Traffic Control Devices". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_350-a.pdf
 - 6) NCHRP Report 475: "A Procedure for Assessing and Planning Nighttime Highway Construction and Maintenance". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_475.pdf
 - 7) NCHRP Report 476: "Guidelines for Design and Operation of Nighttime Traffic Control for Highway Maintenance". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_476.pdf
 - 8) NCHRP Report 498: "Illumination Guidelines for Nighttime Highway Work". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_498.pdf
 - 9) American Association of State Highway and Transportation Officials (AASHTO) Roadside Design Guide.
 - 10) American Traffic Safety Services Association (ATSSA) Quality Guidelines for Work Zone Traffic Control Devices and Features.
 - 11) U.S. Department of Transportation Federal Highway Administration Traffic Control Handbook for Mobile Operations at Night. <http://www.dot.state.il.us/blr/I023.pdf>

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CHANNELIZING DEVICES

- The following devices may be used as channelizing devices: Tubular Markers, Vertical Panels, Cones, Drums and Super Cones.
- 28 inch traffic cones are not allowed on:
 - Interstates
 - Highways with speeds greater than 40 mph.
- During nighttime operations, 28 inch and 36 inch cones are not allowed.
- Retroreflective material pattern used on super cones shall match that used on drums.
- Tangent Areas:**
 - Standard Spacing:** See Standard Device Spacing and Buffer Space table.
 - Daylight Operations:** Drums and super cones are spaced at standard spacing. All other devices are at 1/2 standard spacing.
 - Nighttime Operations:** Drums and supercones at standard spacing are the only devices allowed.
- Taper Areas:**
 - Standard Spacing:** See Standard Device Spacing and Buffer Space table.
 - Daylight Operations:** Drums are spaced at standard spacing. All other devices are 1/2 standard spacing.
 - Nighttime Operations:** Drums (at standard spacing) are the only devices allowed.
- Type C steady burn lights shall be used on all channelizing devices in the taper as well as the first two devices in the tangent at night, (see the AML).
- Typical channelizing device lateral placement (do not include when it is used as a divider for opposing directions of traffic) shall be 2 feet off the lane line in the closed lane or shoulder.
- Devices may be adjusted laterally to accommodate ongoing work in the immediate vicinity but must be returned to the closed lane after the work activity has moved.
- Channelizing devices on the lane line shall be of the same type.
- Channelizing devices in each taper shall be of the same type.

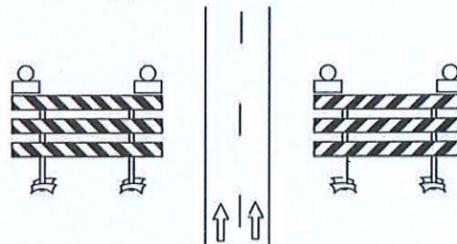


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TYPE III BARRICADES

- Only Type III Barricades shall be used.
- All barricades shall use Type 3 High Intensity Sheeting on both sides of the barricade.
- All barricades shall be a minimum of 8 feet in length and must meet NCHRP Report 350 or MASH requirements.
- When used for overnight closures, two Type B High Intensity Lights shall supplement all barricades that are placed in a closed lane or that extend across a highway. Two Type A Low Intensity Lights may be used in urban areas if approved by the Engineer (See AML).
- When signs and lights are to be mounted to a barricade, they must meet NCHRP Report 350 or MASH requirements.
- A truck with a TMA may be substituted for a barricade when workers are present.
- Barricades shall be placed:
 - at the beginning of a closed lane or shoulder and at 1,000 foot intervals where no active work is ongoing and the lane must remain closed. A minimum of 2 barricades shall be placed if the lane or shoulder closure is less than 2,000 feet. (One barricade shall be placed at the beginning of the lane closure after the buffer space and one shall be placed in the middle of the lane closure.)
 - before each or group of unfilled holes or holes filled with temporary material.
 - before uncured concrete.
 - in the closed lane on each side of every intersection and crossover. (Do not block sight distance.)
 - in front of piles of material (dirt, aggregate, broken concrete), culverts and equipment which is near the work zone.



TTC for DROP-OFFS

NON-INTERSTATE

Average Drop-off	Current Posted Speed (Prior to Construction)	
	> 45 MPH	≤ 45 MPH
≤ 3 IN	Low Shoulder Sign (Optional)	Low Shoulder Sign (Optional)
> 3 IN	Shoulder Drop Off Sign & Edge Lines or Shoulder Drop Off Sign & Channelizing Device	Shoulder Drop Off Sign
> 6 IN	No Shoulder Sign, Edge Lines & Vertical Panel	No Shoulder Sign & Channelizing Device
> 10 IN	Concrete Barrier (if drop off is < 12 FT from edge of travel lane) & Edge Lines	No Shoulder Sign & Vertical Panel

INTERSTATE

Average Drop-off	Low Shoulder Sign (Optional)
≤ 2 IN	Low Shoulder Sign (Optional)
> 2 IN	Shoulder Drop Off Sign & Edge Lines or Shoulder Drop Off Sign & Channelizing Device
> 6 IN	Concrete Barrier (if drop off is < 12 FT from edge of travel lane), Shoulder Drop Off Sign, & Edge Lines

- If a portable concrete barrier will be required then the deflection shall be considered in the design.
- For Interstate ramps, refer to non-Interstate drop offs.



STANDARD DEVICE SPACING AND BUFFER SPACE

SPEED LIMIT (prior to construction) MPH	MERGING TAPER LENGTH (L) Lane Width (FT)				STANDARD DEVICE SPACING IN FEET		BUFFER SPACE FT
	9	10	11	12	Along Taper	Along Tangent	
25	94	105	115	125	20	40	155
30	135	150	165	180	30	60	200
35	184	205	225	245	35	70	250
40	240	267	294	320	40	80	305
45	405	450	495	540	40	80	360
50	450	500	550	600	40	80	425
55	495	550	605	660	40	80	495
60	540	600	660	720	40	80	570
65	585	650	715	780	40	80	645
70	630	700	770	840	40	80	730
75	675	750	825	900	40	80	820

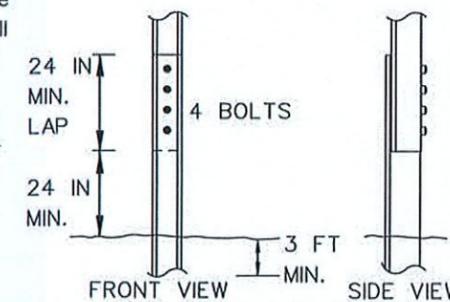
SPEED LIMIT (prior to construction) MPH	SHIFTING TAPER LENGTH (1/2)(L) Lane Shift (FT)						STANDARD DEVICE SPACING IN FEET		BUFFER SPACE FT
	2	4	6	8	10	12	Along Taper	Along Tangent	
25	11	21	32	42	52	63	20	40	155
30	15	30	45	60	75	90	30	60	200
35	21	41	62	82	102	123	35	70	250
40	27	54	80	107	134	160	40	80	305
45	45	90	135	180	225	270	40	80	360
50	50	100	150	200	250	300	40	80	425
55	55	110	165	220	275	330	40	80	495
60	60	120	180	240	300	360	40	80	570
65	65	130	195	260	325	390	40	80	645
70	70	140	210	280	350	420	40	80	730
75	75	150	225	300	375	450	40	80	820

SPEED LIMIT (prior to construction) MPH	SHOULDER TAPER LENGTH (1/3)(L) Shoulder Width (FT)						STANDARD DEVICE SPACING IN FEET		BUFFER SPACE FT
	2	4	6	8	10	12	Along Taper	Along Tangent	
25	7	14	21	28	35	42	20	40	155
30	10	20	30	40	50	60	30	60	200
35	14	28	41	55	68	82	35	70	250
40	18	36	54	72	89	107	40	80	305
45	30	60	90	120	150	180	40	80	360
50	34	67	100	134	167	200	40	80	425
55	37	74	110	147	184	220	40	80	495
60	40	80	120	160	200	240	40	80	570
65	44	87	130	174	217	260	40	80	645
70	47	94	140	187	234	280	40	80	730
75	50	100	150	200	250	300	40	80	820

- All termination and flagger tapers are 100 feet. (MIN. 6 channelizing devices per lane equally spaced 20 feet apart.)
- See TTC Standards for flagger taper.
- See MUTCD for taper formulas.

ALLOWABLE LAP SPICE FOR U-CHANNEL POST

U-Channel posts may be spliced where long lengths are required. The upper section shall overlap the lower section by at least 24 inches. The bottom edge of the upper section of the splice shall be a minimum of 24 inches above the ground. The spliced sections shall be secured with at least four 5/16 inch diameter hex bolts spaced equally along the splice.



SHEET NUMBER 109

ASCENSION

PARISH CONTROL SECTION STATE PROJECT

DESIGNED BY G. LEBLANC CHECKED BY J. COLVIN
 DETAILED BY C. FAQURETT CHECKED BY G. LEBLANC

REVISION OR CHANGE ORDER DESCRIPTION BY DATE

DATE: 7/2/15

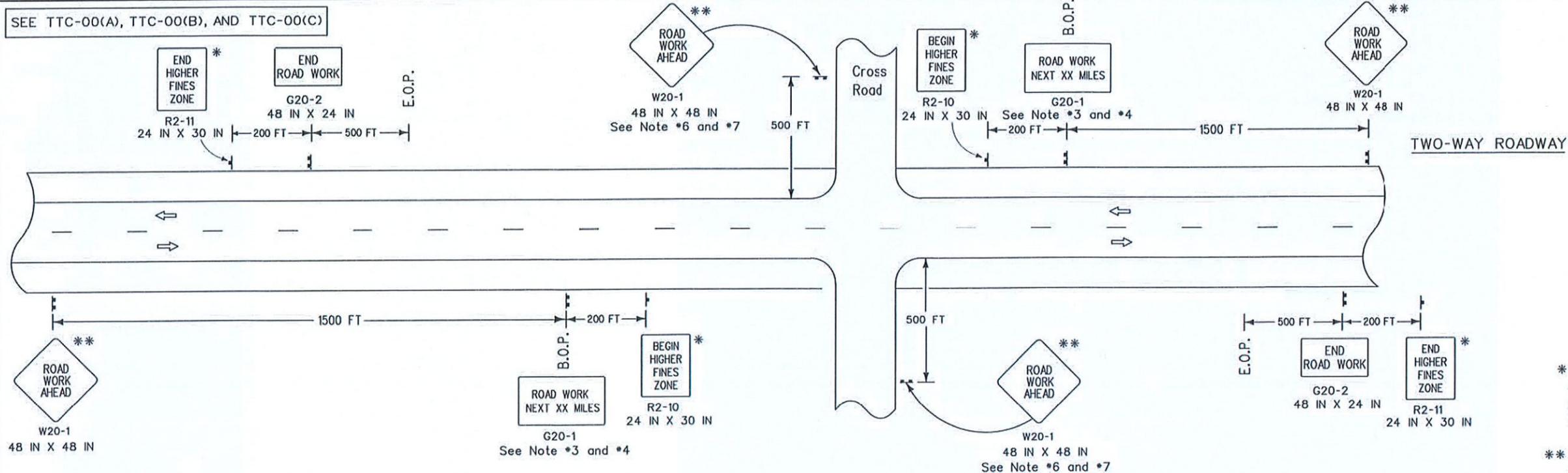
APPROVED BY: *[Signature]* CHIEF ENGINEER

NO. DATE

TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET

TTC-00 (C)

DOT TRAFFIC ENGINEERING



* For divided roadways with speeds \geq 50 mph use larger sign, 36 IN X 48 IN.

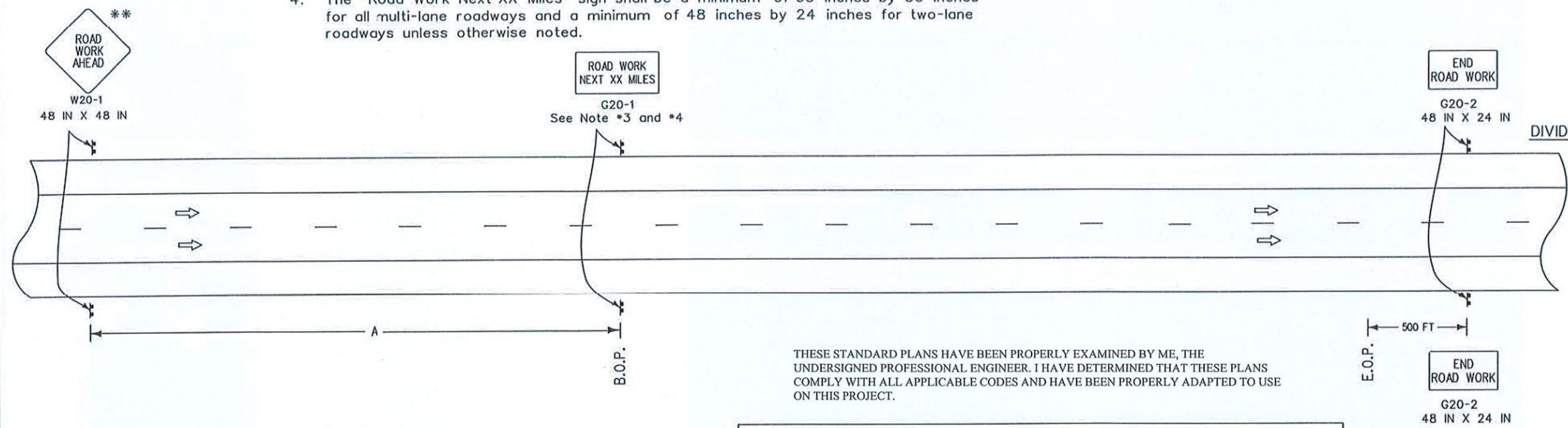
** Any sign of the W20-1 series may be used.

NOTES
 This sheet shall be used with the Temporary Traffic Control General Notes Sheets TTC-00(A), TTC-00(B), TTC-00(C), and other Temporary Traffic Control Sheets as appropriate.

LEGEND

- ▬ Traffic Sign
- ⇒ Direction of Travel

1. This layout represents the minimum traffic controls required for placement of "Road Work Next XX Miles" and "End Road Work" signs.
2. This layout does not replace other TTC Standard Sheets, but is intended as a supplement to the required signing.
3. The distance on the "Road Work Next XX Miles" sign shall be stated to the nearest whole mile. This sign shall be placed at the Beginning of Project (B.O.P.) limits. This sign may be omitted if work zone is less than 0.5 miles.
4. The "Road Work Next XX Miles" sign shall be a minimum of 60 inches by 36 inches for all multi-lane roadways and a minimum of 48 inches by 24 inches for two-lane roadways unless otherwise noted.
5. The "End Road Work" sign shall be placed 500 feet past the End of Project (E.O.P.) limits.
6. If "Road Work Ahead" sign is used on a cross road to warn of road work on another route, then "End Road Work" sign is not required.
7. When projects are separated by less than 1 mile, they shall be signed as one project; this may require coordination.



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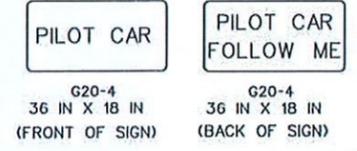
SPEED LIMIT (prior to construction)	SPACING 'A'
≤ 40 mph	1500 FT
45 mph	2640 FT
> 45 mph	5280 FT

- Sign spacing to be adjusted for Horizontal and Vertical curves.
- For work outside of the traveled way, see TTC-01 and TTC-02.

SEE TTC-00(A), TTC-00(B), TTC-00(C), AND TTC-00(D)

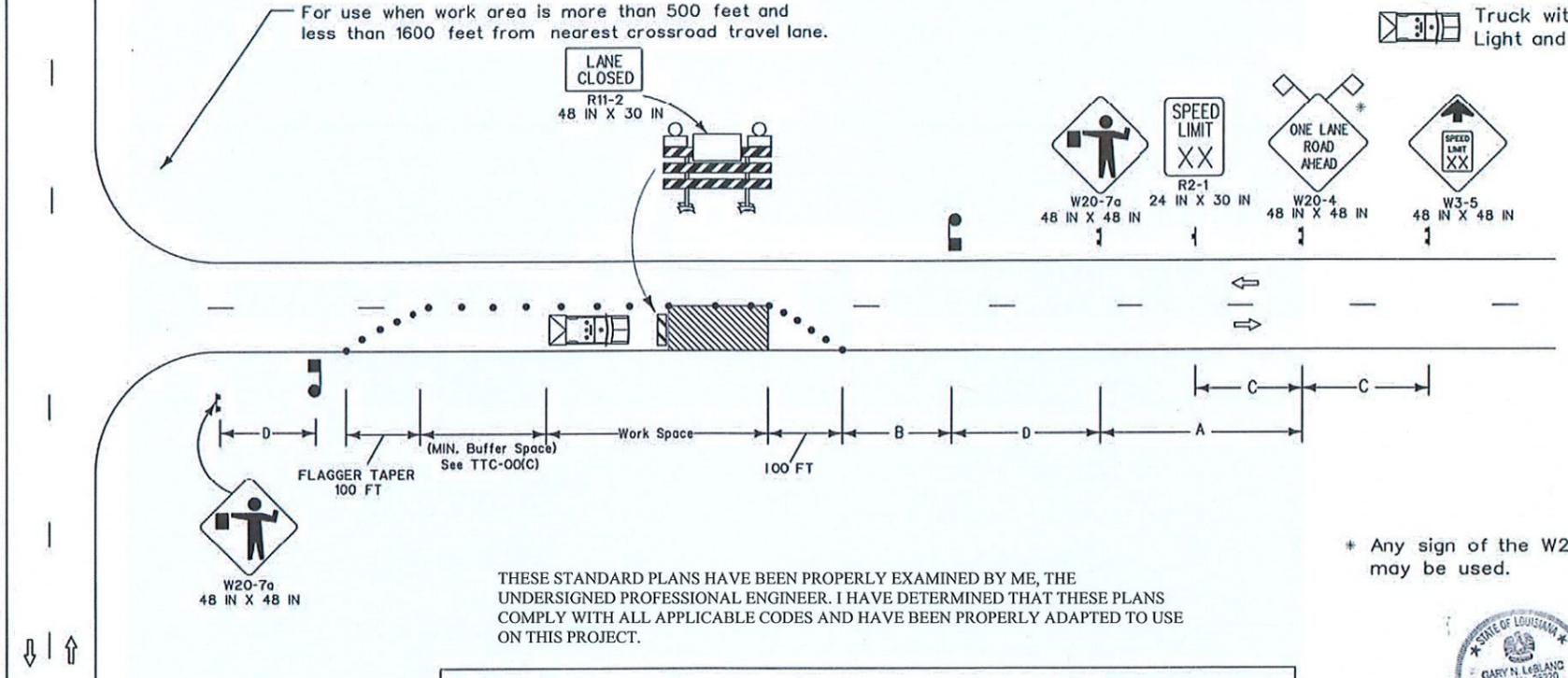
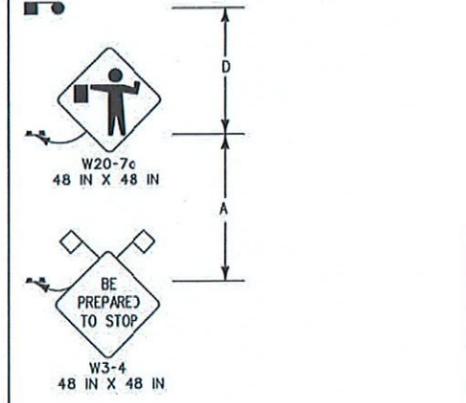
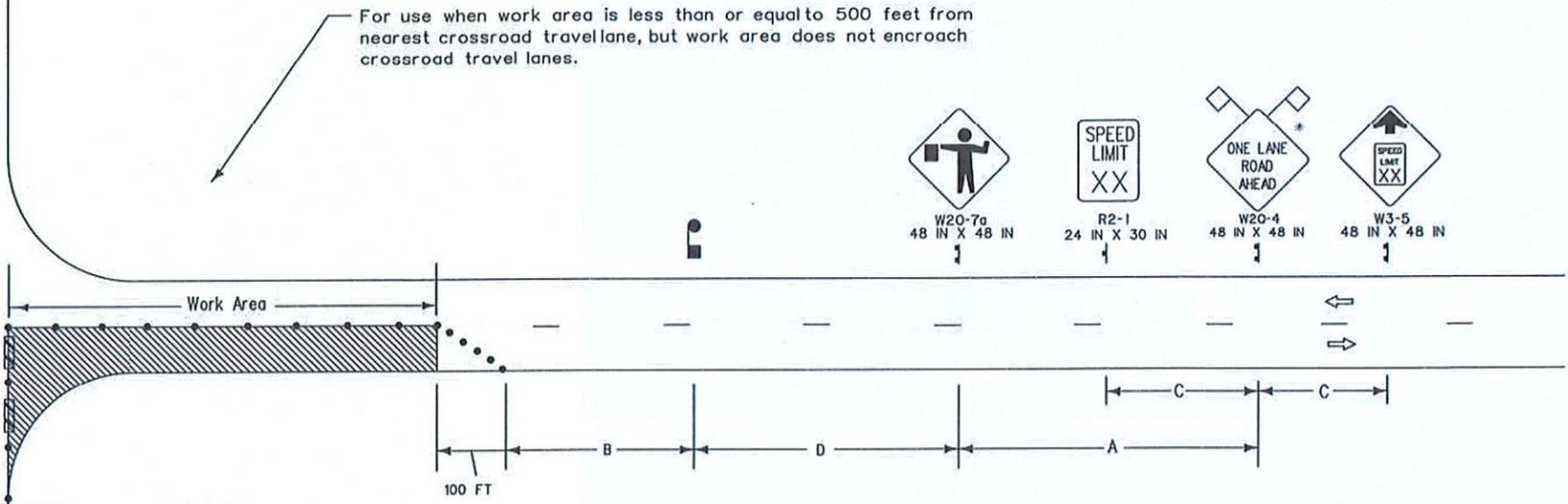
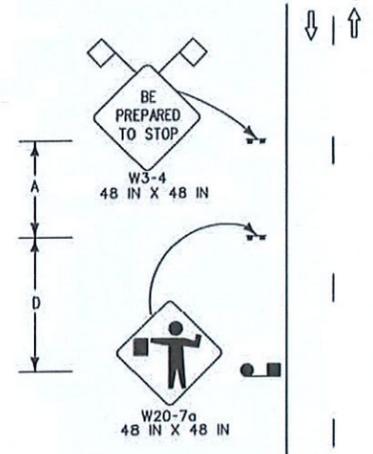
PILOT CAR

- If used, a pilot car shall guide a queue of vehicles through the work zone or diversion.
- It shall be used in restricted visibility operations such as lime or cement stabilization, chip seals, or operations in hilly or curvy terrains, where flaggers cannot see each other (no clear line-of-sight).
- The operation of the pilot vehicle shall be coordinated with flagging operations or other controls at each end of the one-lane section and all major driveways and street intersections.
- The pilot car sign should be mounted 7 feet above roadway in a position visible to oncoming and following traffic.
- The pilot car shall have an amber beacon light.
- The sign mounted on the vehicle shall be two-sided.



- LEGEND**
- ⬇ Traffic Sign
 - Channelizing Devices
 - ▨ Type III Barricades
 - ▨ Work Area
 - 🎵 Flagger
 - 🚚 Type B Light
 - ➡ Direction of Travel
 - 🚚 Truck with Amber Light and TMA

SPEED LIMIT (prior to construction)	SPACING			
	'A'	'B'	'C'	'D'
≤ 40 mph	500 FT	100 FT	N/A	125 FT
45-50 mph	1000 FT	350 FT	500 FT	350 FT
> 50 mph	1500 FT	500 FT	800 FT	500 FT



NOTES
 This sheet shall be used with the Temporary Traffic Control General Notes Sheets TTC-00(A), TTC-00(B), TTC-00(C), and TTC-00(D).

1. This layout represents the minimum traffic controls required for lane closures on two-lane roads with two-way traffic less than 1600 feet from an intersection. For advance signing see TTC-00(D).
2. Visual or radio contact shall be required between flaggers at all times. The flagger shall be visible from flagger sign.
3. Only law officers shall direct traffic against a traffic signal indication.
4. If work area is greater than 1600 feet see TTC-04.
5. If a pilot car is required then the contractor is not required to have channelizing devices in the tangent section.
6. A vehicle with a flashing amber light and a truck mounted attenuator shall be used on all roadways with an ADT greater than 20,000 and a pre-construction speed greater than or equal to 40 mph. This vehicle shall move with work operations not to exceed the roll-ahead distance required by the manufacturer plus 100 feet.

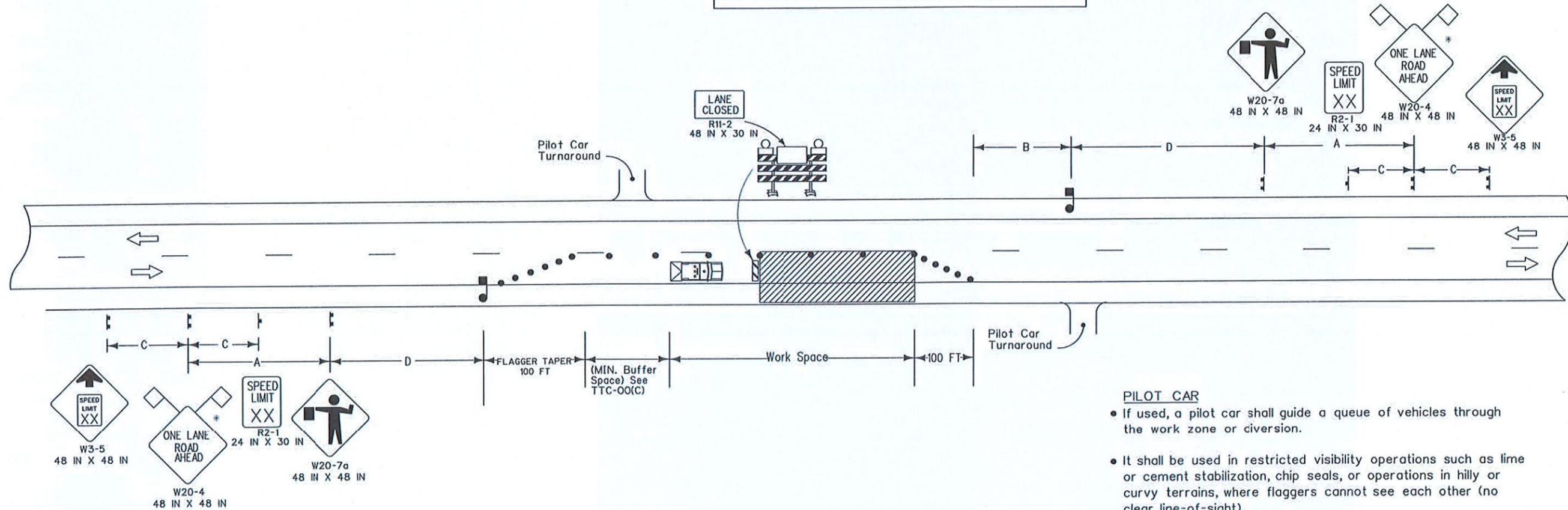
THESE STANDARD PLANS HAVE BEEN PROPERLY EXAMINED BY ME, THE UNDERSIGNED PROFESSIONAL ENGINEER. I HAVE DETERMINED THAT THESE PLANS COMPLY WITH ALL APPLICABLE CODES AND HAVE BEEN PROPERLY ADAPTED TO USE ON THIS PROJECT.

ALL TTC STANDARDS SHOW MINIMUM CONSTRUCTION SIGNING. ALL SITUATIONS SHALL BE REVIEWED AND/OR DESIGNED BY THE ENGINEER. CONTRACTORS ARE RESPONSIBLE FOR COMPLYING WITH ALL TTC STANDARDS.

* Any sign of the W20-4 series may be used.

GARY N. LEBLANC
 REG. NO. 28220
 REGISTERED PROFESSIONAL ENGINEER
 IN CIVIL ENGINEERING
 6-27-18

SEE TTC-00(A), TTC-00(B), TTC-00(C), AND TTC-00(D)



NOTES

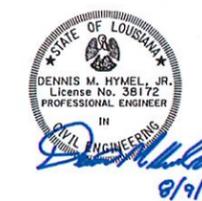
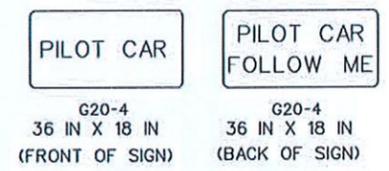
This sheet shall be used with the Temporary Traffic Control General Notes Sheets TTC-00(A), TTC-00(B), TTC-00(C) and TTC-00(D).

1. This layout represents the minimum traffic controls required for lane closures on two-lane roads with two-way traffic greater than 1600 feet from an intersection. For this type of closure either a flagger or a pilot car will be required. For advance signing see TTC-00(D).
2. To prevent vehicles from entering the work area against the flow of traffic, an additional flagger shall be stationed at each intersection, major driveway, railroad crossing, or crossing within the work area.
3. For projects in rural areas the distance between flaggers shall not exceed:
 - (A) 2.5 miles for ADT < 2,500
 - (B) 2.0 miles for 2,500 < ADT < 5,000
 - (C) 1.5 miles for ADT > 5,000
4. The flagger station shall be near the beginning of the taper and shall have adequate sight distance to be visible to oncoming traffic. If sight distance cannot be achieved, the distance between flaggers may be extended for a short duration.
5. Visual or radio contact shall be required between flaggers at all times. The flagger shall be visible from the flagger sign.
6. A vehicle with a flashing amber light and a truck mounted attenuator shall be used on all roadways with an ADT greater than 20,000 and a pre-construction speed greater than or equal to 40 mph. This vehicle shall move with work operations not to exceed the roll-ahead distance required by the manufacturer plus 100 feet.

7. If a pilot car is required then the contractor is not required to have channelizing devices in the tangent section.
8. If work zone is less than 1600 feet from an intersection see TTC-03.

PILOT CAR

- If used, a pilot car shall guide a queue of vehicles through the work zone or diversion.
- It shall be used in restricted visibility operations such as lime or cement stabilization, chip seals, or operations in hilly or curvy terrains, where flaggers cannot see each other (no clear line-of-sight).
- The operation of the pilot vehicle shall be coordinated with flagging operations or other controls at each end of the one-lane section and all major driveways and street intersections.
- The pilot car sign should be mounted 7 feet above roadway in a position visible to oncoming and following traffic.
- The pilot car shall have an amber beacon light.
- The sign mounted on the vehicle shall be two-sided.



SPEED LIMIT (prior to construction)	SPACING			
	'A'	'B'	'C'	'D'
≤ 40 mph	500 FT	100 FT	N/A	125 FT
45-50 mph	1000 FT	350 FT	500 FT	350 FT
≥ 55 mph	1500 FT	500 FT	800 FT	500 FT

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LEGEND

- Traffic Sign
- Flagger
- Channelizing Devices
- Type III Barricades
- Work Area
- Type B Light
- Direction of Travel
- Truck with Amber Light and TMA

* Any sign of the W20-4 series may be used.

SHEET NUMBER	112	ASCESSION	
DESIGNED	G. LEBLANC	PARISH	ASCENSION
CHECKED	J. COLVIN	CONTROL SECTION	
DATE		STATE	LA
PROJECT		PROJECT	
DETAILER	C. FAYOURI	SERIES NUMBER	
CHECKED	G. LEBLANC	DATE	7/2/18
REVISION OR CHANGE ORDER DESCRIPTION		APPROVED BY	
		CHIEF ENGINEER	
TEMPORARY TRAFFIC CONTROL LAYOUT FOR LANE CLOSURES ON TWO LANE ROADS WITH TWO-WAY TRAFFIC (FLAGGING OPERATIONS)			
TTC-04			